



Meeting Agenda

Plan and Zoning Commission

Tuesday, September 17, 2024

6:30 PM

Ankeny City Council Chambers

1250 SW District Drive, Second Floor, Ankeny, Iowa

Ted Rapp, Chair

Randy Weisheit, Vice Chair

Trina Flack
Glenn Hunter

Lisa West

Annette Renaud
Todd Ripper

Plan and Zoning Commission regular meetings are held at 6:30 p.m. on the first and third Tuesdays of each month, following the Monday City Council meetings. All Plan and Zoning meetings are open to the public. Assistive Listening Devices (ALD) are available for persons with impaired hearing.

CALL TO ORDER:

A. ROLL CALL:

B. AMENDMENTS TO AGENDA:

Consider MOTION to approve and accept the September 17, 2024 agenda with/without amendments.

C. COMMUNICATIONS:

D. CITIZEN'S REQUEST:

E. CONSENT AGENDA ITEMS:

1. Minutes

Consider MOTION to approve and accept the September 4, 2024 minutes of the Plan and Zoning Commission meeting.

2. Kimberley Villas - Updated Building Elevations

Consider MOTION to approve the building elevations for Kimberley Villas.

3. 4703 N Ankeny Boulevard - Ankeny Community Baptist Church Parking Expansion Site Plan

Consider MOTION to approve the site plan for 4703 N Ankeny Boulevard, Ankeny Community Baptist Church Parking Expansion.

4. Deer Creek Rowhomes Plat 1 Final Plat

Consider MOTION to recommend City Council approval of Deer Creek Rowhomes Plat 1 Final Plat.

5. Deer Creek Rowhomes Site Plan

Consider MOTION to approve the site plan for Deer Creek Rowhomes, subject to recordation of the final plat.

Consider MOTION to approve the recommendations for Consent Agenda Item(s) #1 - #5.

F. REMOVED CONSENT AGENDA ITEMS:

G. REPORTS:

1. September 16, 2024 City Council Report - Staff
2. Director's Report
Tentative agenda items for Tuesday, October 8, 2024
3. Commissioner's Reports

H. MISCELLANEOUS ITEMS:

October 7, 2024 - 5:30 p.m. City Council Representative: Staff

I. ADJOURNMENT:

Consider MOTION to adjourn the meeting.



PLAN AND ZONING COMMISSION
September 17, 2024
6 : 30 PM

Print

ORIGINATING DEPARTMENT:
Planning and Building

COUNCIL GOAL:
Ensure Economic Vitality

ACTION REQUESTED:
Motion

LEGAL:

SUBJECT:

Minutes

EXECUTIVE SUMMARY:

FISCAL IMPACT: **No**

STAFF RECOMMENDATIONS:

PREVIOUS COUNCIL/COMMISSION/BOARD ACTION(S):

PUBLIC OUTREACH EFFORTS:

ACTION REQUESTED:

Action: Consider MOTION to approve and accept the September 4, 2024 minutes of the Plan and Zoning Commission meeting.

ADDITIONAL INFORMATION:

ATTACHMENTS:

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Meeting Minutes
Plan & Zoning Commission Meeting
Wednesday, September 4, 2024
Ankeny City Council Chambers, Second Floor
1250 SW District Drive, Ankeny, Iowa

CALL TO ORDER

Chair Ted Rapp called the September 4, 2024 meeting of the Plan & Zoning Commission to order at 6:30 p.m.

ROLL CALL

Members present: Ted Rapp, Trina Flack, Glenn Hunter, Lisa West, and Todd Ripper. Absent: Annette Renaud and Randy Weisheit. Staff present: Eric Jensen, Eric Carstens, Bryan Morrissey, Laura Hutzell, and Brenda Fuglsang.

AMENDMENTS TO THE AGENDA

Motion by T.Flack to approve and accept the September 4, 2024 agenda without amendments. Second by L.West. All voted aye. Motion carried 5 – 0.

COMMUNICATIONS

There were no communications.

CITIZEN'S REQUEST

There were no requests.

CONSENT AGENDA ITEMS

Item #1. Minutes

Motion to approve and accept the August 20, 2024 minutes of the Plan and Zoning Commission meeting.

Item #2. 3710 SE Convenience Boulevard - Rick's Landing Airplane Hangar Site Plan

Motion to approve the site plan for the Rick's Landing Airplane Hangar located at 3710 SE Convenience Boulevard.

Item #3. Trestle Ridge Estates Plat 8 Final Plat

Motion to recommend City Council approval of Trestle Ridge Estates Plat 8 Final Plat; and recommend City cost-participation in the amount of \$9,796.50 for 8' sidewalk along NW Reinhart Drive. An estimate for the cost of the turn lanes along NW Weigel Drive will be reviewed and approved by the City. Once there is an agreement on the cost, the developer will provide a check for the required amount at the time of final plat approval, which will be applied to the future City project.

Motion by T.Flack to approve the recommendations for Consent Agenda Item #1 - #3. Second by G.Hunter. All voted aye. Motion carried 5 – 0.

PUBLIC HEARINGS

There were no public hearings.

BUSINESS ITEMS

There were no business items.

REPORTS

City Council Meeting

E.Jensen reported on the September 3, 2024 City Council meeting.

Director's Report

E.Jensen presented the tentative agenda items for the Wednesday, September 4, 2024 Plan and Zoning Commission meeting and the August 2024 Building Permits Report.

Commissioner's Reports

There were no reports.

MISCELLANEOUS ITEMS

Introduction to CivicClerk Agenda Board Portal.

September 16, 2024 – 5:30 p.m. City Council Meeting Representative: Staff

ADJOURNMENT

There being no further business, Chair Rapp adjourned the meeting. Meeting adjourned at 7:05 p.m.



Submitted by Brenda Fuglsang, Secretary
Plan & Zoning Commission



PLAN AND ZONING COMMISSION
September 17, 2024
6 : 30 PM

Print

ORIGINATING DEPARTMENT:
Planning and Building

COUNCIL GOAL:
Ensure Economic Vitality

ACTION REQUESTED:
Motion

LEGAL:
No Review Required

SUBJECT:
Kimberley Villas - Updated Building Elevations

EXECUTIVE SUMMARY:
Staff report attached.

FISCAL IMPACT: **No**

STAFF RECOMMENDATIONS:

PREVIOUS COUNCIL/COMMISSION/BOARD ACTION(S):
Plan and Zoning Commission recommended approval of the site plan for Kimberley Villas on December 5, 2023.

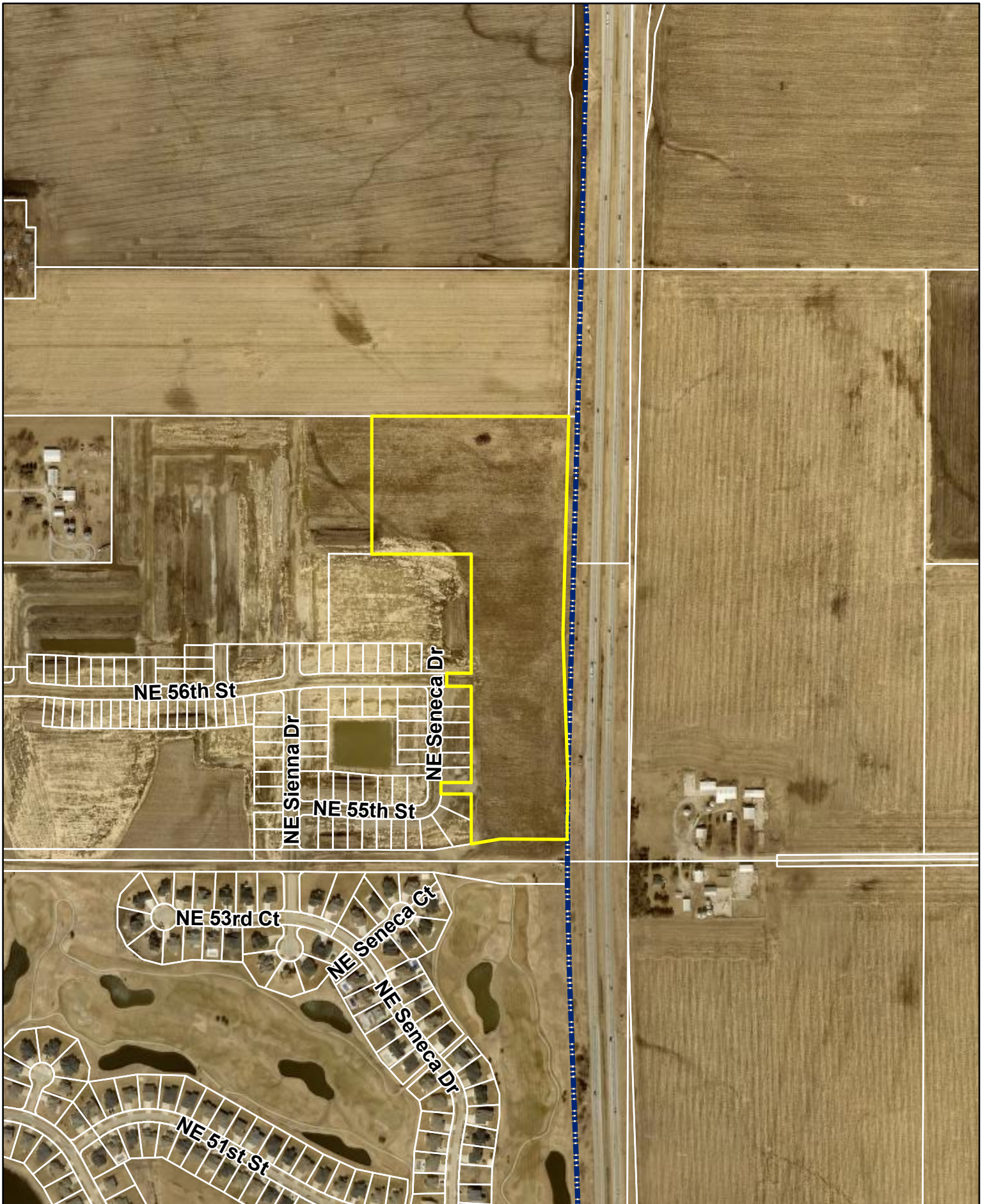
PUBLIC OUTREACH EFFORTS:

ACTION REQUESTED:
Action: Consider MOTION to approve the building elevations for Kimberley Villas.

ADDITIONAL INFORMATION:

ATTACHMENTS:

Click to download
Aerial Map
Staff Report
Applicant Letter
Proposed Elevations
Approved Elevations



N



1 inch = 550 feet

Date: 11/30/2023

Kimberley Villas - Site Plan



*Plan and Zoning Commission
Staff Report*

Meeting Date: September 17, 2024

Agenda Item: Kimberley Villas – Updated Building Elevations
Report Date: September 12, 2024
Prepared by: Bryan Morrisey *EJC*
Associate Planner

Staff Recommendation:

That the Plan and Zoning Commission approve the updated building elevations for the Kimberley Villas Site Plan.

Project Summary:

A site plan for the Kimberley Villas development was approved by the Plan & Zoning Commission on December 5, 2023. The approved site plan outlined 225 attached townhome units located north of NE 54th Street and west of Interstate-35 within the northeastern quadrant of Ankeny.

The original building elevations for the townhome units that were approved with the site plan showed substantial amounts of permanent materials (masonry) for units that were visible from the public streets that surrounded the development. The permanent materials were included to ensure that the primary material requirements of the Ankeny City Code were being met. Due to a law that was recently passed by the State of Iowa, material requirements for these kind of townhome developments are no longer enforceable by municipalities.

The updated building elevations that are now being proposed show a significant reduction in the permanent materials that were shown on the original elevations. The majority of the townhome units will be comprised primarily of board and batten and lap siding, with a minimal amount of masonry included on the front of the units. The Sydney elevations will be utilized on Plats 2-4 of Kimberley Villas while the Pattison elevations will be utilized on Plat 1. The color scheme for the overall development will largely be the same to avoid a monotonous and repetitive design.

Ankeny Commission:

DR Horton is providing you with new plans for the Kimberley Villa Project in Ankeny Iowa. Changes are as listed:

- We will now do a 2ft stone wrap on the garage front of the unit. Note: XT02 will have stone wrapped on the side of the garage as well
- We do a blend of board and batten and shake on the most front façade/pitch to provide architectural distinction
- We will now do LAP siding down the side and back of the units

Lastly, we still plan to use the color package below and layout as referenced in our original submittal. We hope for your approval to drive for affordability in this coming development. Look forward working with you in the future!

Sincerely,

Anastacia Smith

Land Acquisition

DR Horton

NO.	DATE	REVISION
1	08.22.24	

PROFESSIONAL SEAL:

PROJECT TITLE:
Townhomes Master Set 'Sydney' v2.0

CLIENTS NAME:

D-R HORTON
America's Builder

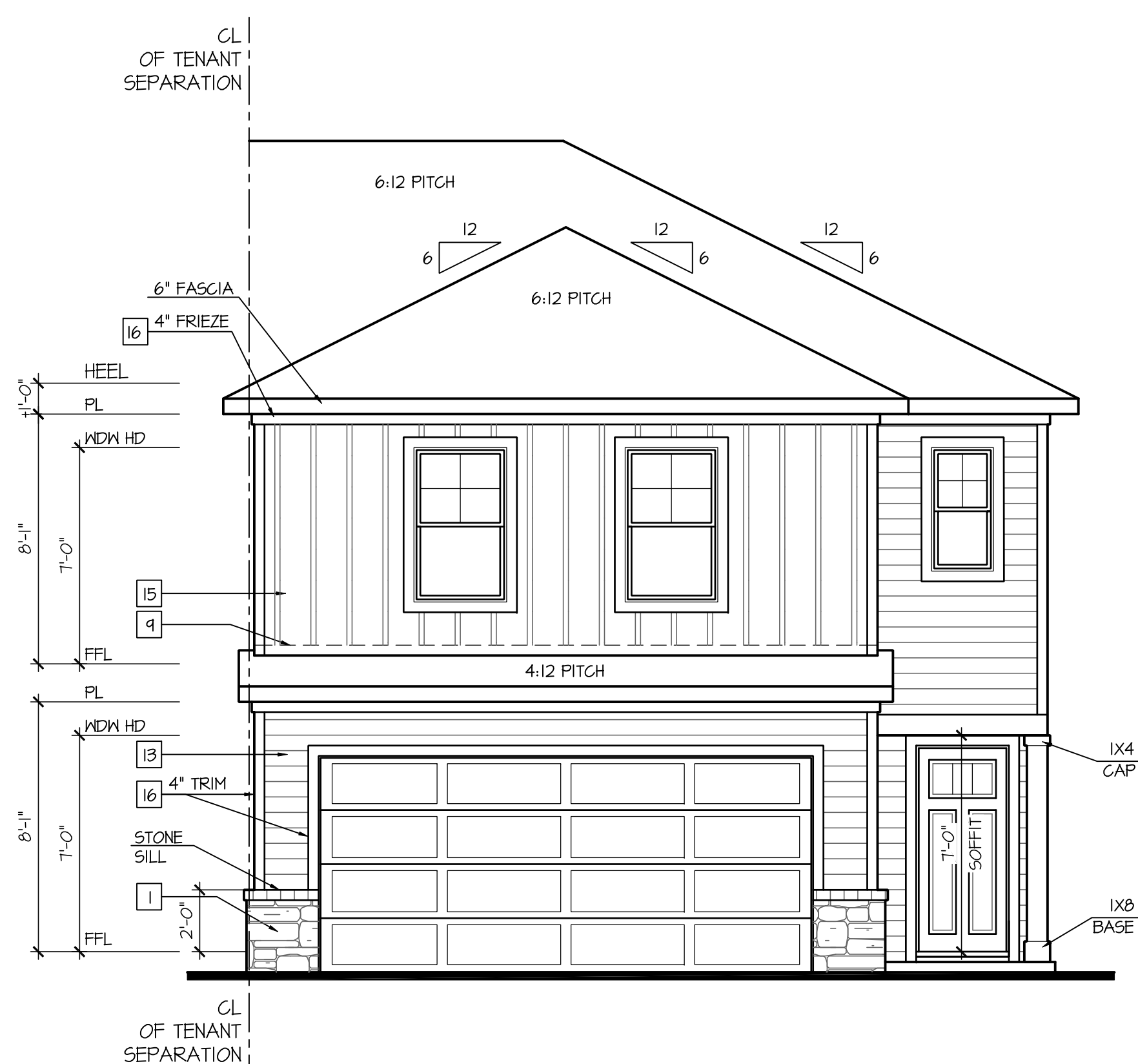
PROJECT NO: GMD-GA20076

SHEET TITLE:
SYDNEY EXTERIOR ELEVATIONS 'B'

PRINT DATE:
 June 02, 2021

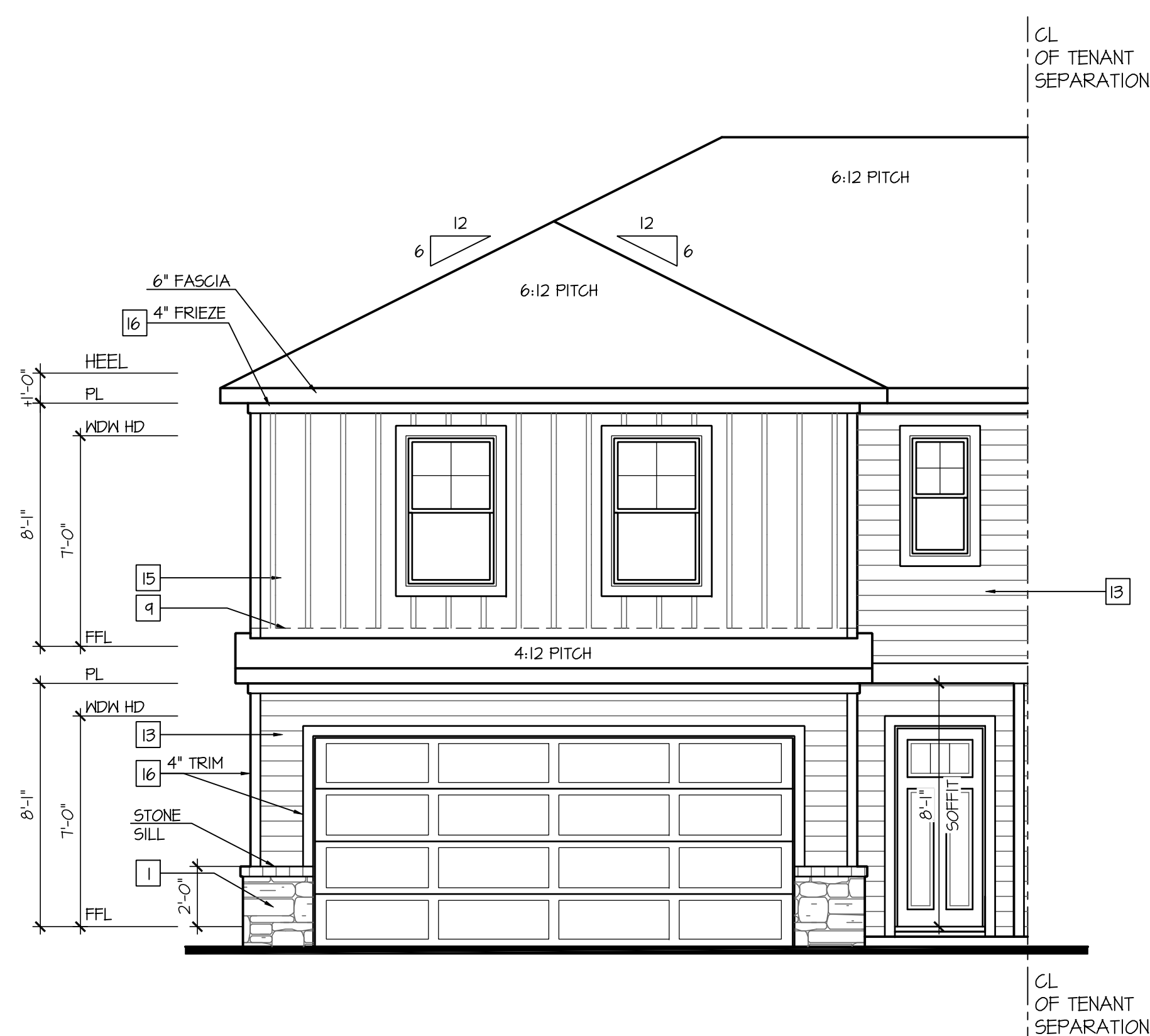
SHEET NO:
A6.0

FOR CONSTRUCTION



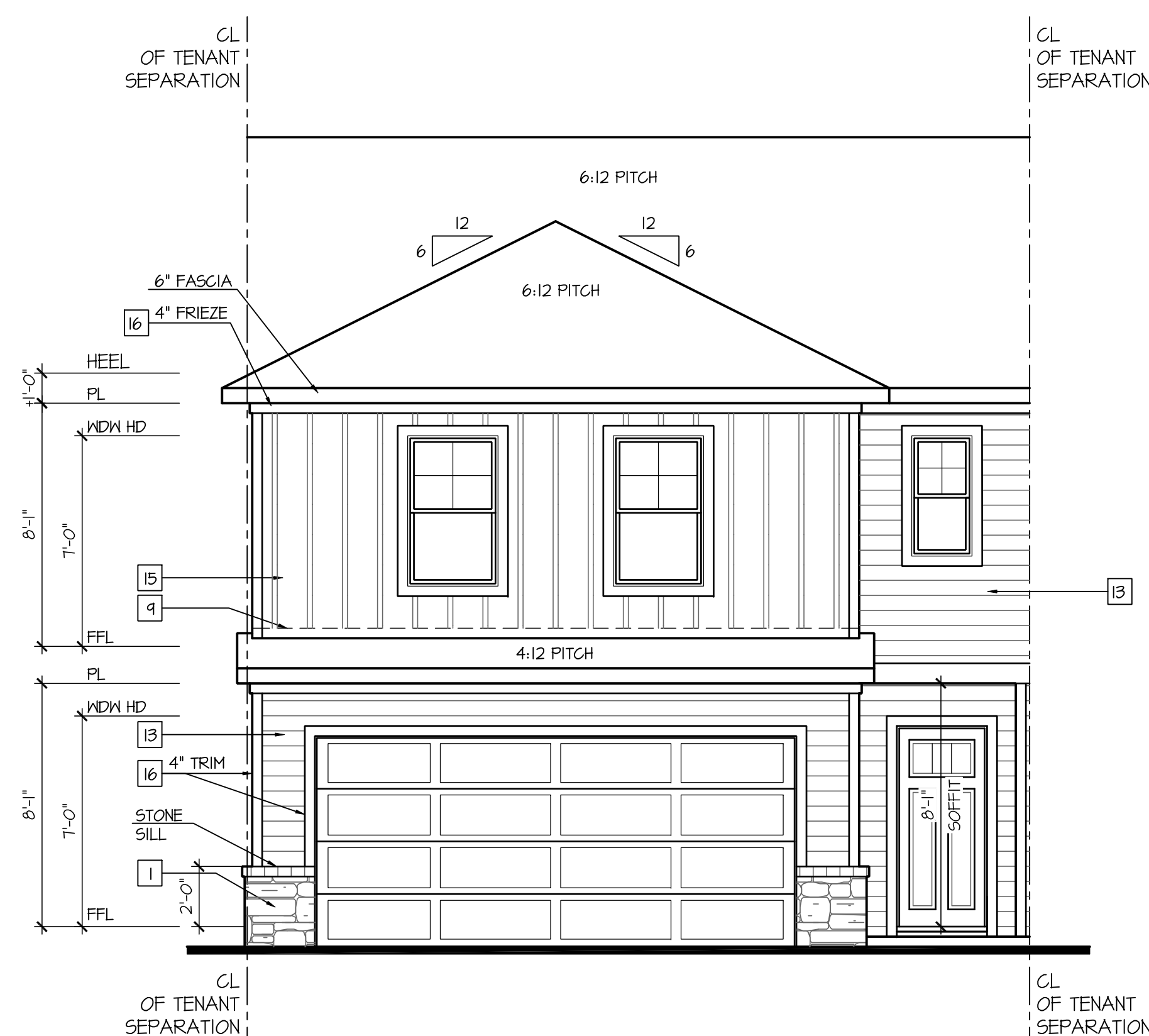
MATERIAL CALCULATIONS:	
	SQ FT
LAP SIDING	124
B4B	128
SHAKE	0
MASONRY	12
	PERCENTAGE
LAP SIDING	46.91
B4B	48.48
SHAKE	0.00
MASONRY	4.56

Front Elevation 'A-1'
At End Unit Condition - R
 SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X11" LAYOUT



MATERIAL CALCULATIONS:	
	SQ FT
LAP SIDING	124
B4B	128
SHAKE	0
MASONRY	12
	PERCENTAGE
LAP SIDING	46.91
B4B	48.48
SHAKE	0.00
MASONRY	4.56

Front Elevation 'A-1'
At End Unit Condition - L
 SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X11" LAYOUT



MATERIAL CALCULATIONS:	
	SQ FT
LAP SIDING	124
B4B	128
SHAKE	0
MASONRY	12
	PERCENTAGE
LAP SIDING	46.91
B4B	48.48
SHAKE	0.00
MASONRY	4.56

Front Elevation 'A-1'
At Interior Unit Condition
 SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X11" LAYOUT

NOTES:

- GRADE CONDITIONS MAY VARY FOR INDIVIDUAL SITE FROM THAT SHOWN. BUILDER SHALL VERIFY AND COORDINATE PER ACTUAL SITE CONDITIONS.
- WINDOW HEAD HEIGHTS:
 1ST FLOOR = 7'-0" U.N.O. ON ELEVATIONS.
 2ND FLOOR = 7'-0" U.N.O. ON ELEVATIONS.
- ROOFING: PITCHED SHINGLES PER DEVELOPER.
- WINDOWS: MANUFACTURER PER DEVELOPER. DIVIDED LITES AS SHOWN ON THE EXTERIOR ELEVATIONS
- ENTRY DOOR: AS SELECTED BY DEVELOPER.
- GARAGE DOORS: AS SELECTED BY DEVELOPER, RAISED PANEL AS SHOWN.
- CHIMNEY AS OCCURS: TOP OF CHIMNEYS TO BE A MINIMUM OF 24" ABOVE ANY ROOF WITHIN 10'-0" OF CHIMNEY.
- ALL EXTERIOR MATERIALS TO BE INSTALLED PER MANUFACTURER'S WRITTEN INSTRUCTIONS.
- PROTECTION AGAINST DECAY:
 (ALL PORTIONS OF A PORCH, SCREEN PORCH OR DECK FROM THE BOTTOM OF THE HEADER DOWN, INCLUDING POST, RAILS, PICKETS, STEPS AND FLOOR STRUCTURE)
- SHINGLED ROOF PITCHES FROM 2/12 TO 4/12 TO HAVE (2) LAYERS OF UNDERLAYMENT APPLIED AND OVERLAPPED PER R405.2.1.

KEY NOTES:

MASONRY:

- ADHERED STONE VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.
- MASONRY FULL BRICK AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.
- ADHERED BRICK VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.
- 8" SOLDIER COURSE.
- ROWLOCK COURSE.
- DECORATIVE KEY. SEE DETAIL.

TYPICALS:

- CORROSION RESISTANT SCREEN LOWERED VENTS, SIZE AS NOTED.
- CODE APPROVED TERMINATION CHIMNEY CAP.
- CORROSION RESISTANT ROOF TO WALL FLASHING. CODE COMPLIANT FLASHING MUST BE INSTALLED AT ALL ROOF/WALL INTERSECTIONS.
- STANDING SEAM METAL ROOF, INSTALL PER MANUFACTURER'S WRITTEN INSTRUCTIONS.
- ROOFING FINISH, PER SPECS

SIDING:

- SHAKE SIDING PER DEVELOPER
- LAP SIDING PER DEVELOPER
- N/A
- PANEL SIDING W/ 1X3 BATTS AT 12" O.C. (BATT SIDING)
- 1X TRIM OR EQUAL, U.N.O. SIZE AS NOTED
- SHUTTERS, TYPE AS SHOWN. SIZE AS NOTED.

ALL WINDOWS WHOSE OPENING IS LESS THAN 24" ABOVE THE FINISH FLOOR AND WHOSE OPENING IS GREATER THAN 12" ABOVE THE OUTSIDE WALKING SURFACE MUST HAVE WINDOW OPENING LIMITING DEVICES COMPLYING WITH LOCAL CODES

NO.	DATE	REVISION
1	08.22.24	

PROFESSIONAL SEAL:

PROJECT TITLE:
Townhomes Master Set 'Sydney' v2.0

CLIENTS NAME:

D-R HORTON
America's Builder

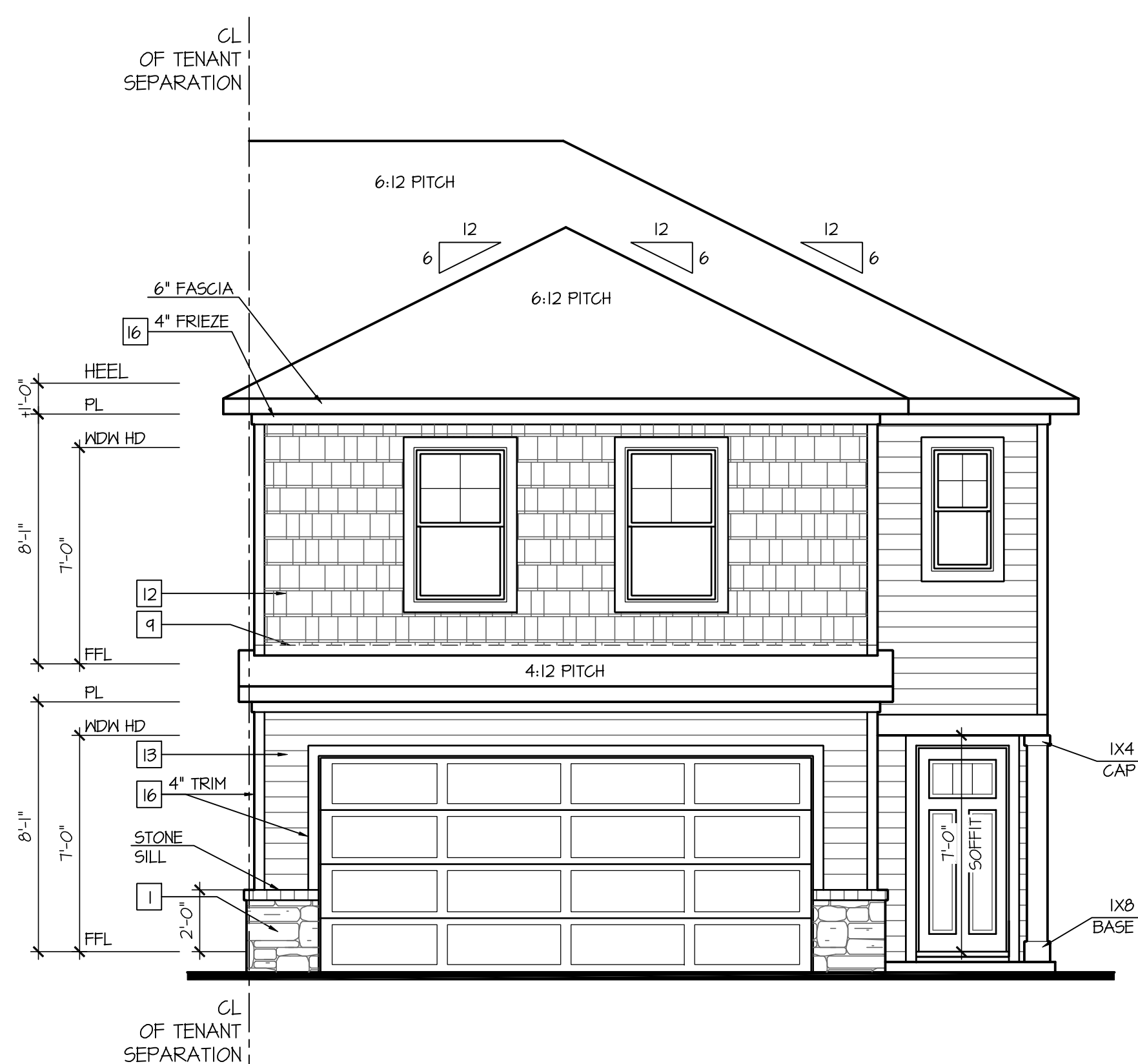
PROJECT NO: GMD-GA20076

SHEET TITLE:
SYDNEY EXTERIOR ELEVATIONS 'A'

PRINT DATE:
 June 02, 2021

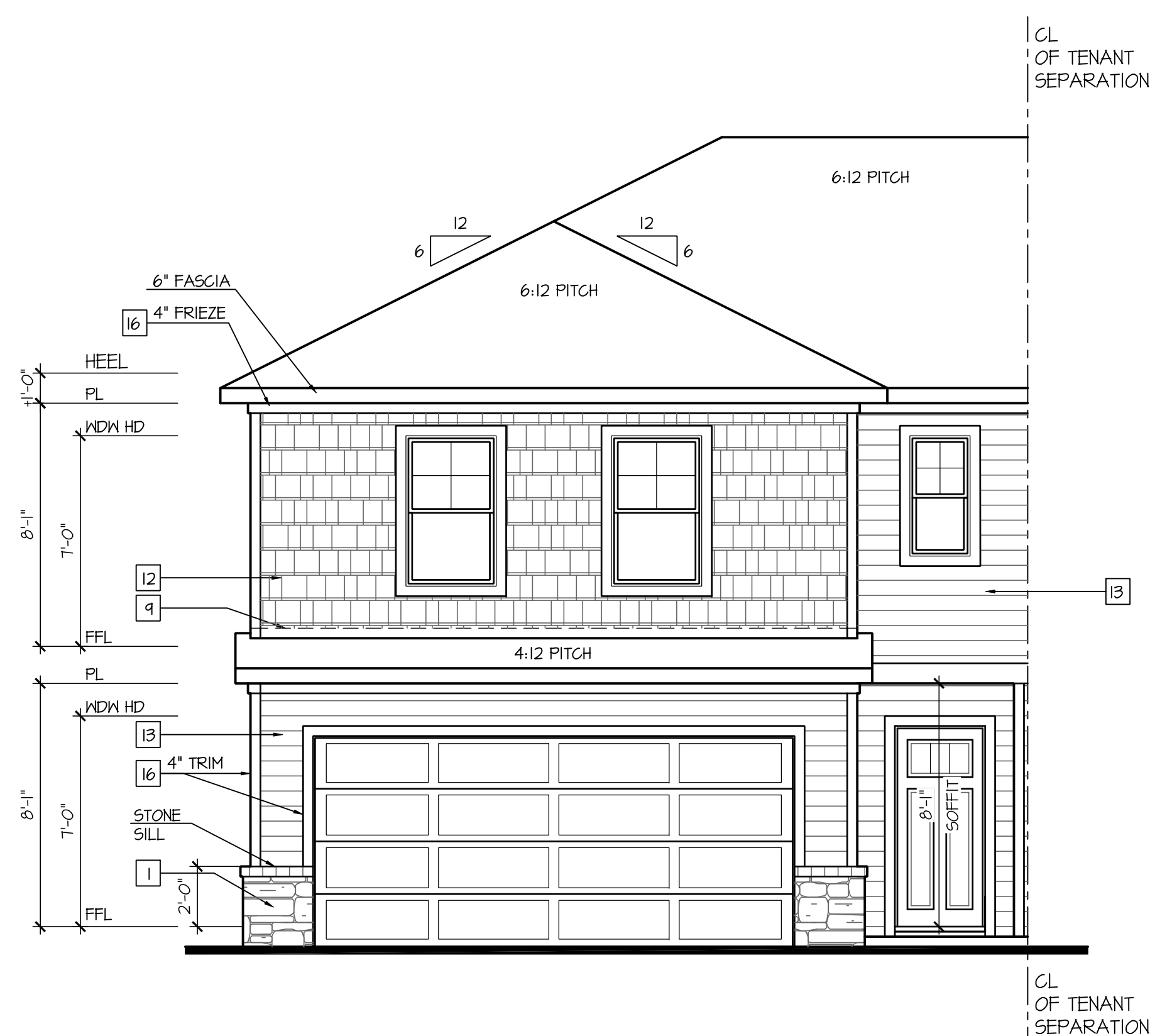
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A6.1

FOR CONSTRUCTION



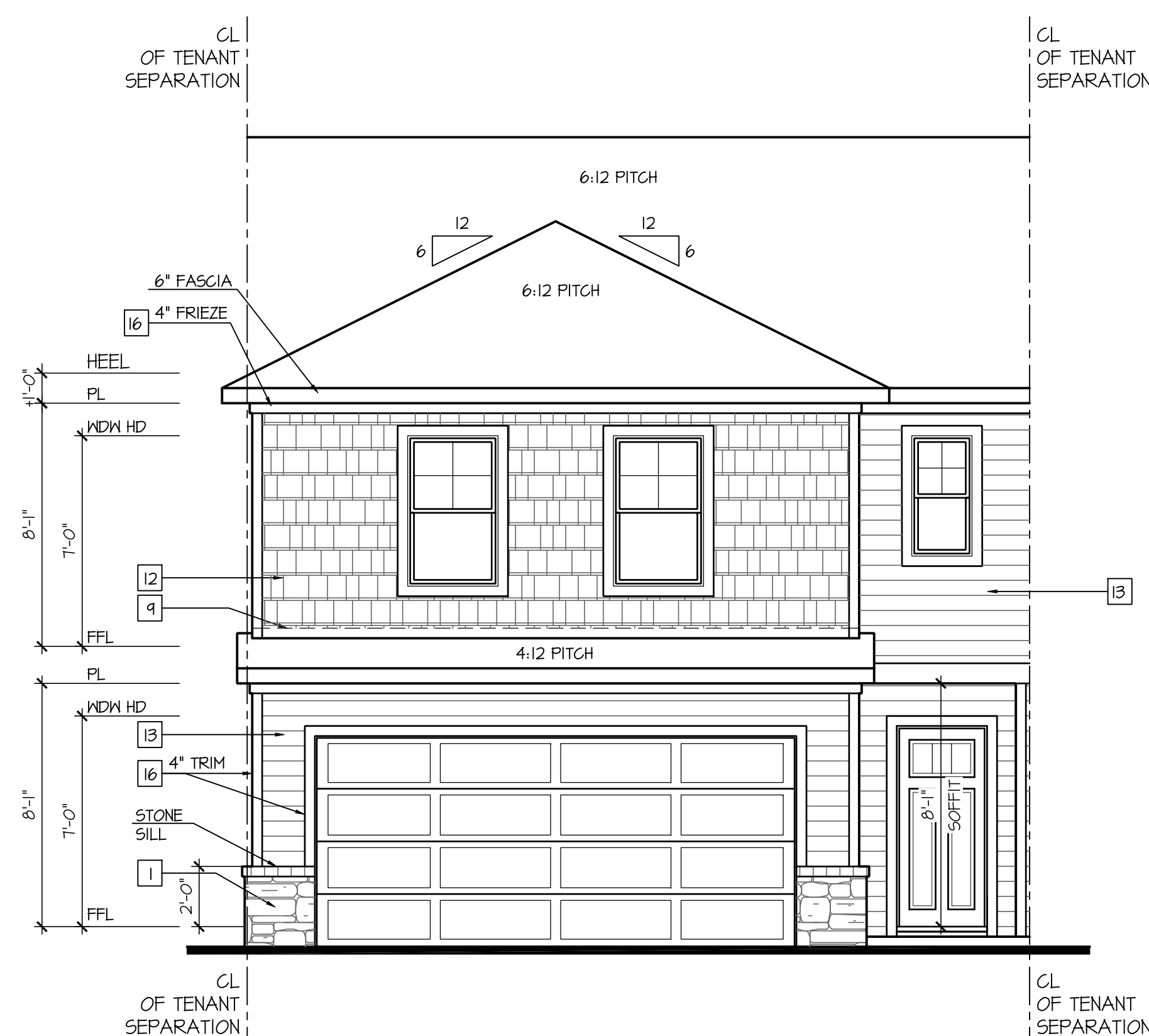
MATERIAL CALCULATIONS:	
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LAP SIDING	124
B4B	0
SHAKE	128
MASONRY	12
	PERCENTAGE
LAP SIDING	46.91
B4B	0.00
SHAKE	48.88
MASONRY	4.56

Front Elevation 'A-2'
At End Unit Condition - R
 SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X11" LAYOUT



MATERIAL CALCULATIONS:	
	SQ FT
LAP SIDING	124
B4B	0
SHAKE	128
MASONRY	12
	PERCENTAGE
LAP SIDING	46.91
B4B	0.00
SHAKE	48.88
MASONRY	4.56

Front Elevation 'A-2'
At End Unit Condition - L
 SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X11" LAYOUT



MATERIAL CALCULATIONS:	
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B4B	0
SHAKE	128
MASONRY	12
	PERCENTAGE
LAP SIDING	46.91
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SHAKE	48.88
MASONRY	4.56

Front Elevation 'A-2'
At Interior Unit Condition
 SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X11" LAYOUT

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KEY NOTES:

MASONRY:

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- ADHERED BRICK VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.
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TYPICALS:

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- ROOFING FINISH, PER SPECS

SIDING:

- SHAKE SIDING PER DEVELOPER
- LAP SIDING PER DEVELOPER
- N/A
- PANEL SIDING W/ 1X3 BATTS AT 12" O.C. (BATT SIDING)
- 1X TRIM OR EQUAL, U.N.O. SIZE AS NOTED
- SHUTTERS, TYPE AS SHOWN. SIZE AS NOTED.

ALL WINDOWS WHOSE OPENING IS LESS THAN 24" ABOVE THE FINISH FLOOR AND WHOSE OPENING IS GREATER THAN 12" ABOVE THE OUTSIDE WALKING SURFACE MUST HAVE WINDOW OPENING LIMITING DEVICES COMPLYING WITH LOCAL CODES

NOTES:

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2ND FLOOR = 7'-0" U.N.O. ON ELEVATIONS.
- ROOFING: COMPOSITE PITCHED SHINGLE ROOFING SYSTEM PER DEVELOPER.
- WINDOWS: MANUFACTURER PER DEVELOPER. DIVIDED LITES AS SHOWN ON THE EXTERIOR ELEVATIONS.
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KEY NOTES:

MASONRY:

- 1 ADHERED STONE VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.
- 2 MASONRY FULL BRICK AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.
- 3 ADHERED BRICK VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.
- 4 8" SOLDIER COURSE.
- 5 RAINLOCK COURSE
- 6 DECORATIVE KEY. SEE DETAIL.

TYPICALS:

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- 8 CODE APPROVED TERMINATION CHIMNEY CAP.
- 9 CORROSION RESISTANT ROOF TO WALL FLASHING. CODE COMPLIANT FLASHING MUST BE INSTALLED AT ALL ROOF/WALL INTERSECTIONS.

- 10 STANDING SEAM METAL ROOF, INSTALL PER MANUFACTURER'S WRITTEN INSTRUCTIONS.

- 11 ROOFING FINISH, PER SPECS

SIDING:

- 12 SHAKE SIDING PER DEVELOPER
- 13 LAP SIDING PER DEVELOPER

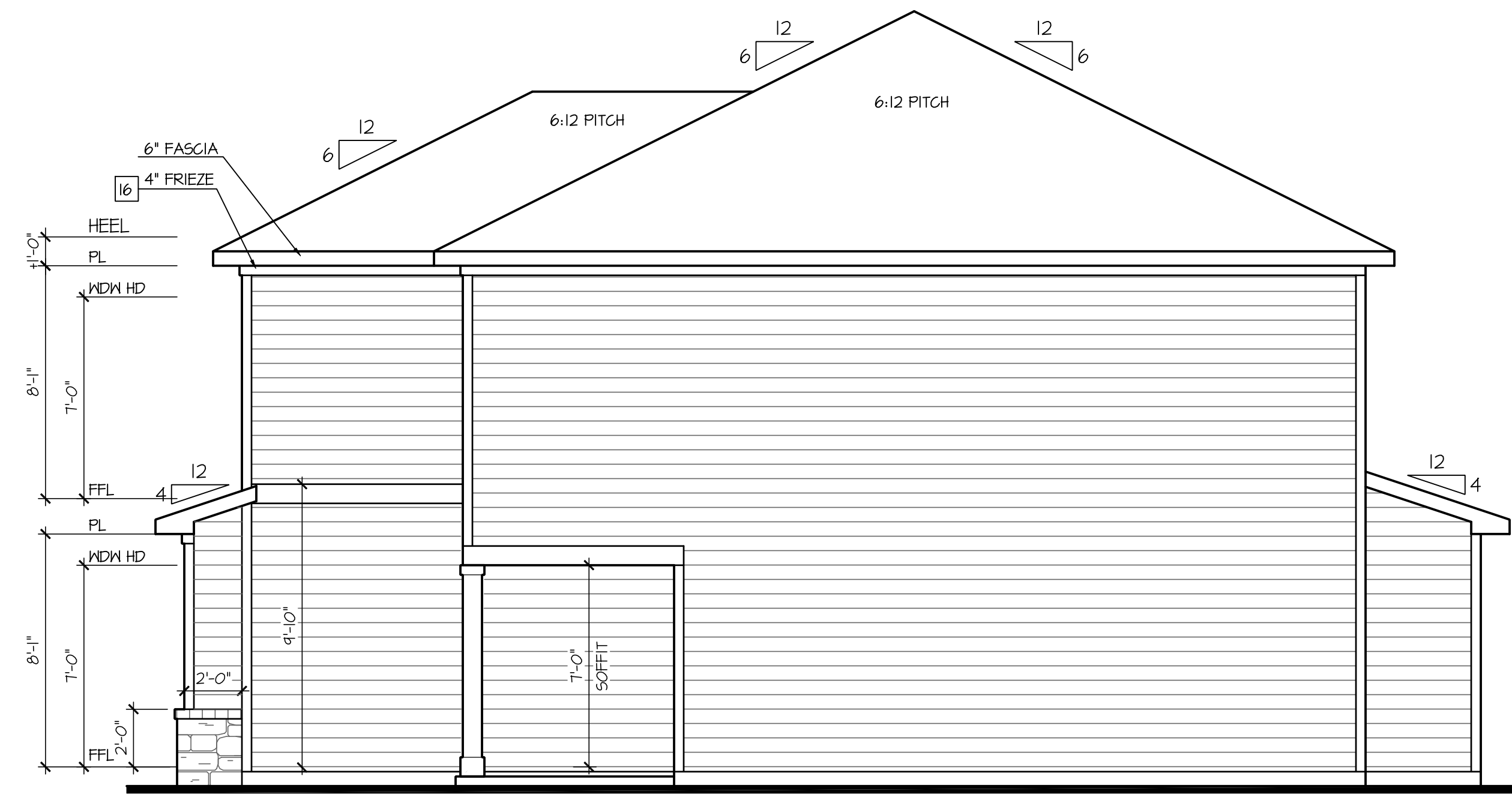
- 14 N/A

- 15 PANEL SIDING W/ 1X3 BATTS AT 12" O.C.
(BATT SIDING)

- 16 1X TRIM OR EQUAL, U.N.O. SIZE AS NOTED

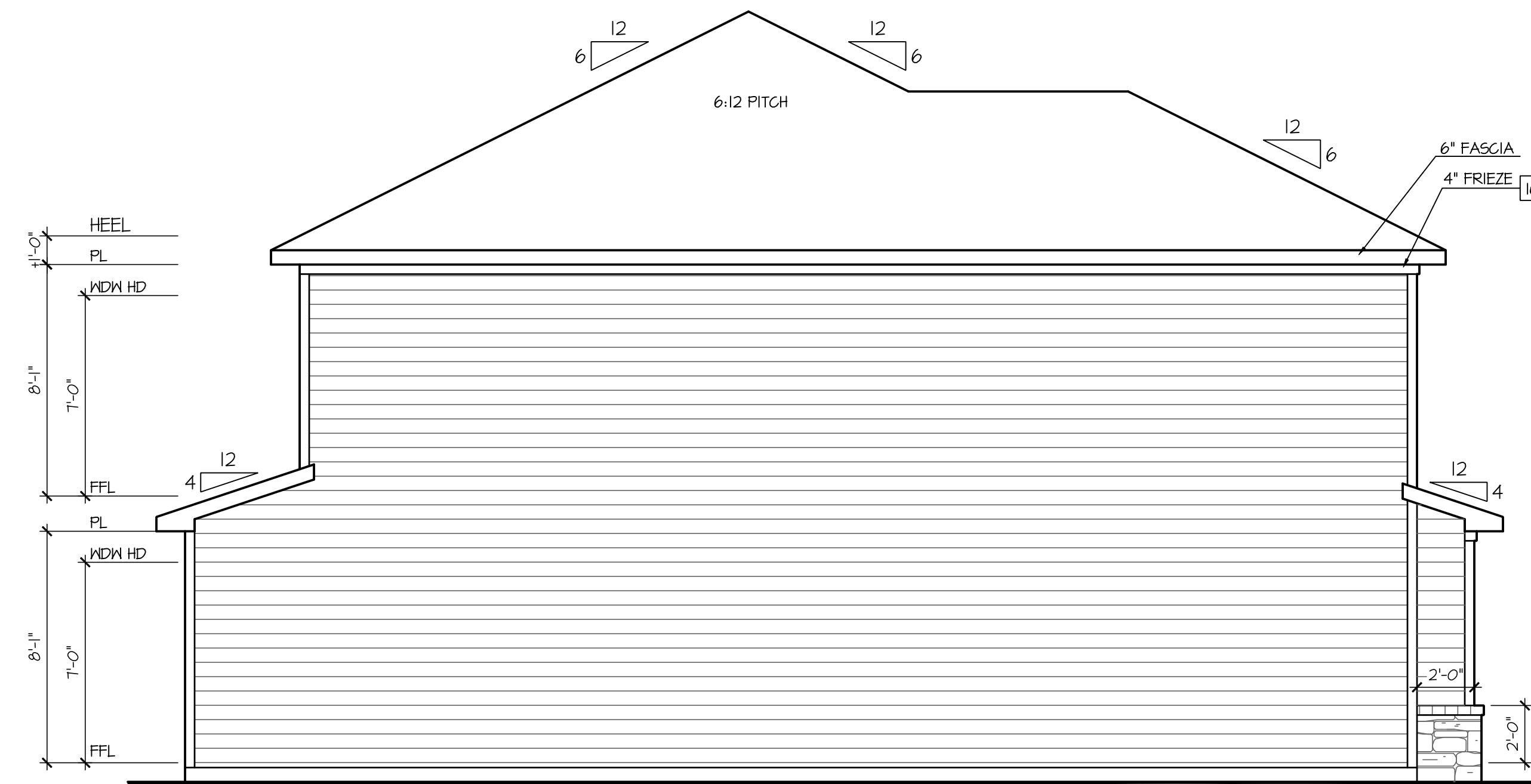
- 17 SHUTTERS, TYPE AS SHOWN. SIZE AS NOTED.

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Right Elevation 'A-1'/'A-2'
At End Condition - R

SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X17" LAYOUT



Left Elevation 'A-1'/'A-2'
At End Condition - L

SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X17" LAYOUT

NO.	DATE	REVISION
1	08.22.24	

PROFESSIONAL SEAL:

PROJECT TITLE:

**Townhomes
Master Set
'Sydney'
v2.0**

**FOR
CONSTRUCTION**

CLIENTS NAME:

D-R-HORTON
America's Builder

PROJECT NO: GMD-GA20076

SHEET TITLE:

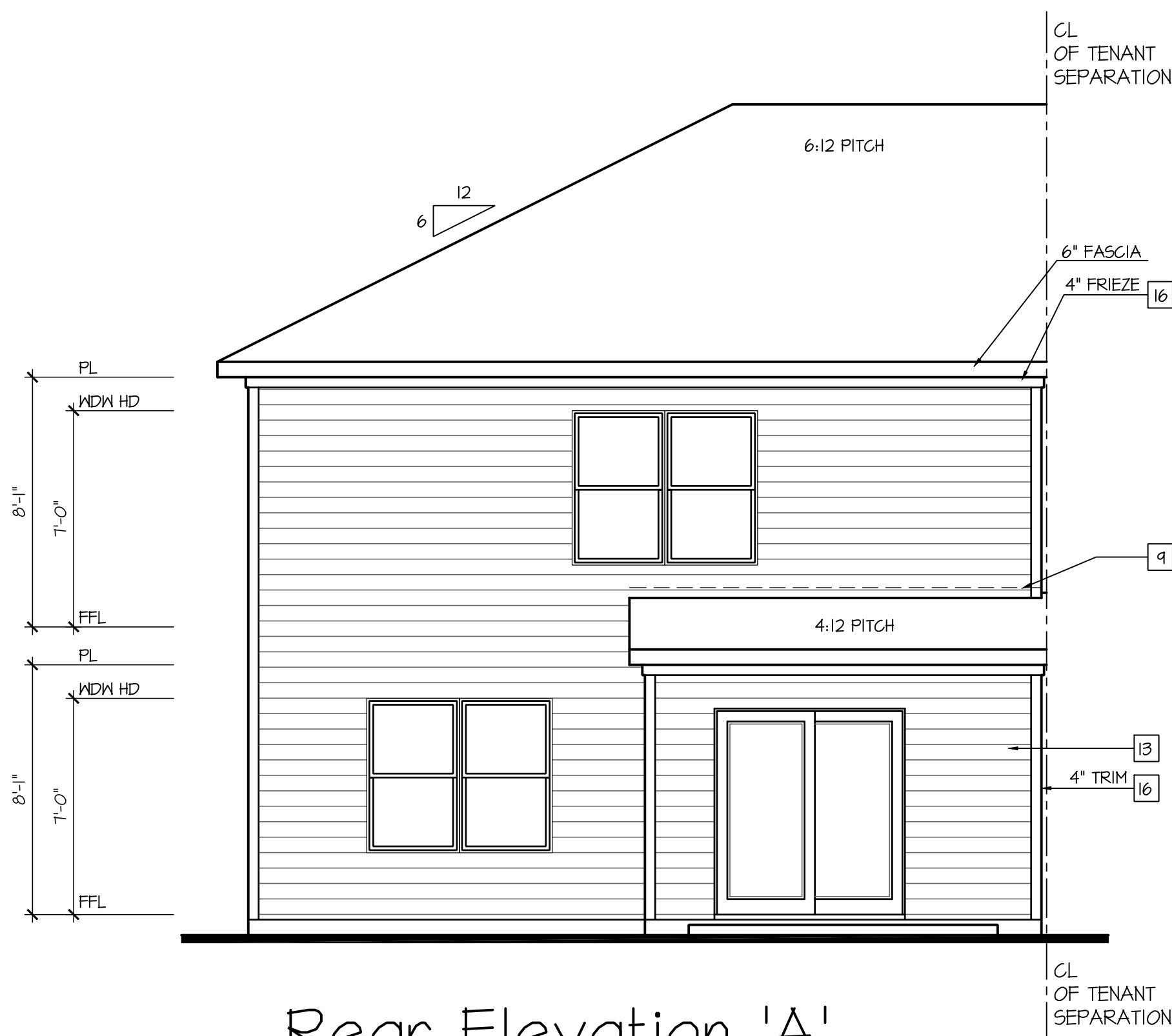
**SYDNEY
EXTERIOR
ELEVATIONS 'A'**

PRINT DATE:

June 02, 2021

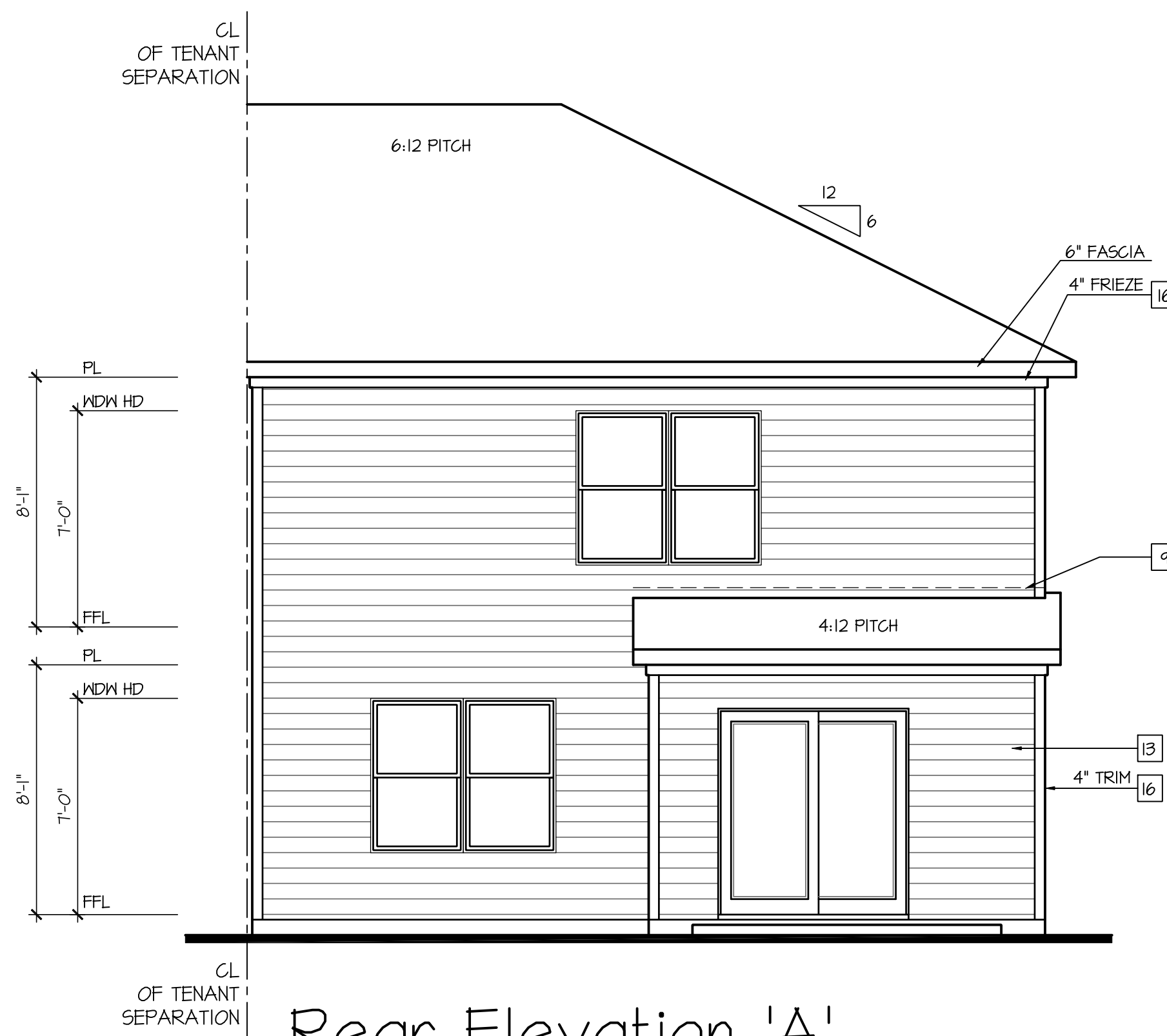
SHEET NO:

A6.4



Rear Elevation 'A'
At End Unit Condition - R

SCALE: 1/4"=1'-0" AT 22'X34" LAYOUT 1/8"=1'-0" AT 11'X11" LAYOUT
AT ELEVATIONS 'A-1', 'A-2', 'A-3' AND 'A-4'



Rear Elevation 'A'
At End Unit Condition - L

SCALE: 1/4"=1'-0" AT 22'X34" LAYOUT 1/8"=1'-0" AT 11'X11" LAYOUT
AT ELEVATIONS 'A-1', 'A-2', 'A-3' AND 'A-4'



Rear Elevation 'A'
At Interior Unit Condition

SCALE: 1/4"=1'-0" AT 22'X34" LAYOUT 1/8"=1'-0" AT 11'X11" LAYOUT
AT ELEVATIONS 'A-1', 'A-2', 'A-3' AND 'A-4'

- NOTES:**
- GRADE CONDITIONS MAY VARY FOR INDIVIDUAL SITE FROM THAT SHOWN. BUILDER SHALL VERIFY AND COORDINATE PER ACTUAL SITE CONDITIONS.
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1ST FLOOR = 7'-0" U.N.O. ON ELEVATIONS.
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 - SHINGLED ROOF PITCHES FROM 2/12 TO 4/12 TO HAVE (2) LAYERS OF UNDERLAYMENT APPLIED AND OVERLAPPED PER R905.2.1.

- KEY NOTES:**
- | | |
|--|---|
| MASONRY: | SIDING: |
| [1] ADHERED STONE VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED. | [2] SHAKE SIDING PER DEVELOPER |
| [2] MASONRY FULL BRICK AS SELECTED BY DEVELOPER. HEIGHT AS NOTED. | [3] LAP SIDING PER DEVELOPER |
| [3] ADHERED BRICK VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED. | [4] N/A |
| [4] 8" SOLDIER COURSE. | [5] PANEL SIDING W/ 1X3 BATTS AT 12" O.C. (BATT SIDING) |
| [5] ROWLOCK COURSE | [6] 1X TRIM OR EQUAL, U.N.O. SIZE AS NOTED |
| [6] DECORATIVE KEY. SEE DETAIL. | [7] SHUTTERS, TYPE AS SHOWN. SIZE AS NOTED. |
| TYPICALS: | |
| [1] CORROSION RESISTANT SCREEN LOUVERED VENTS, SIZE AS NOTED. | |
| [2] CODE APPROVED TERMINATION CHIMNEY CAP. | |
| [3] CORROSION RESISTANT ROOF TO WALL FLASHING. CODE COMPLIANT FLASHING MUST BE INSTALLED AT ALL ROOF/WALL INTERSECTIONS. | |
| [4] STANDING SEAM METAL ROOF, INSTALL PER MANUFACTURER'S WRITTEN INSTRUCTIONS. | |
| [5] ROOFING FINISH, PER SPECS | |
- ALL WINDOWS WHOSE OPENING IS LESS THAN 24" ABOVE THE FINISH FLOOR AND WHOSE OPENING IS GREATER THAN 12" ABOVE THE OUTSIDE WALKING SURFACE MUST HAVE WINDOW OPENING LIMITING DEVICES COMPLYING WITH LOCAL CODES

NO:	DATE:	REVISION:
1	08.22.24	

PROFESSIONAL SEAL:

PROJECT TITLE:

**Townhomes
Master Set
'Sydney'
v2.0**

**FOR
CONSTRUCTION**

CLIENTS NAME:

D-R-HORTON
America's Builder

PROJECT NO: GMD-GA20076

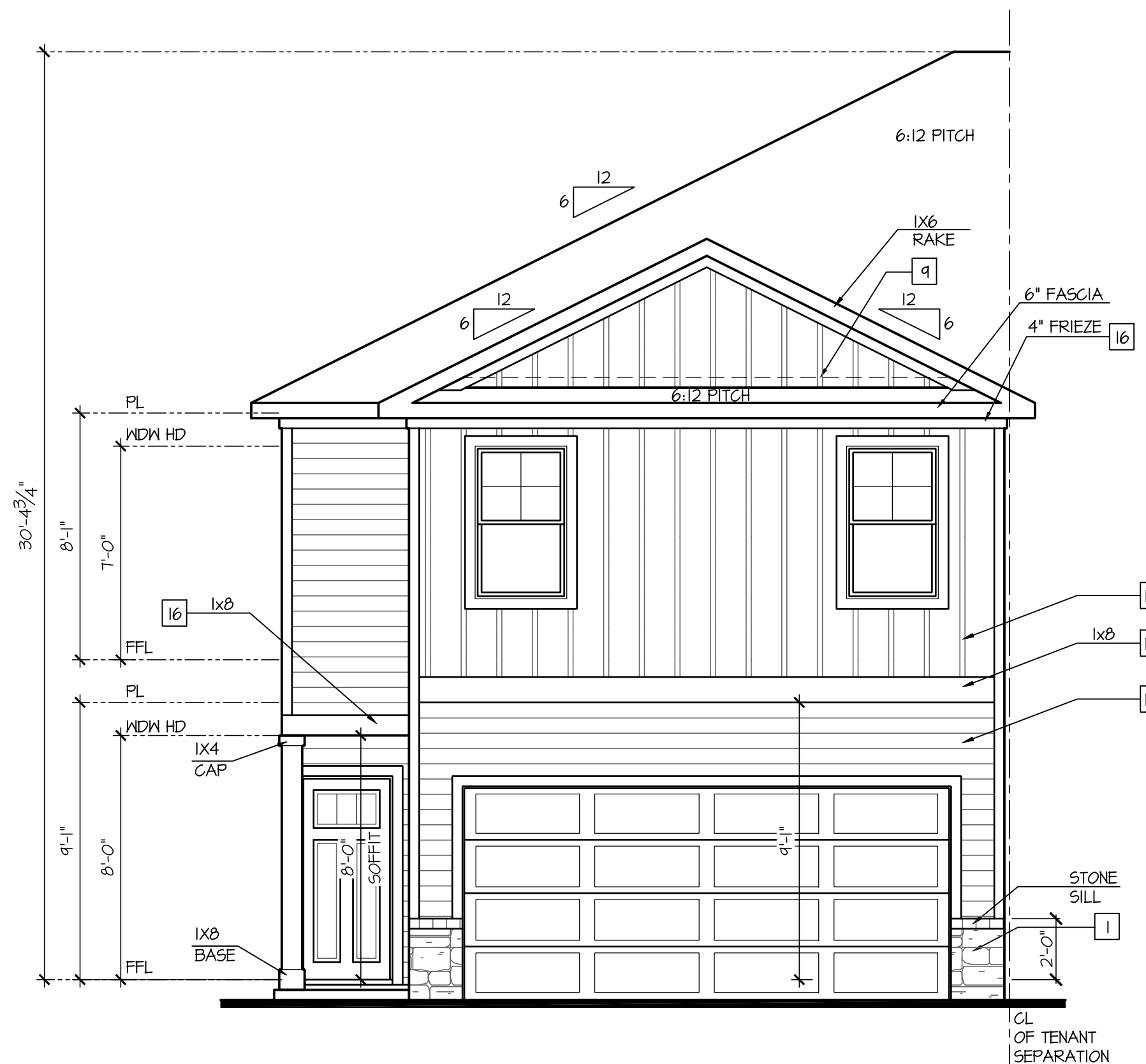
SHEET TITLE:
**SYDNEY
EXTERIOR
ELEVATIONS 'A'**

PRINT DATE:

June 02, 2021

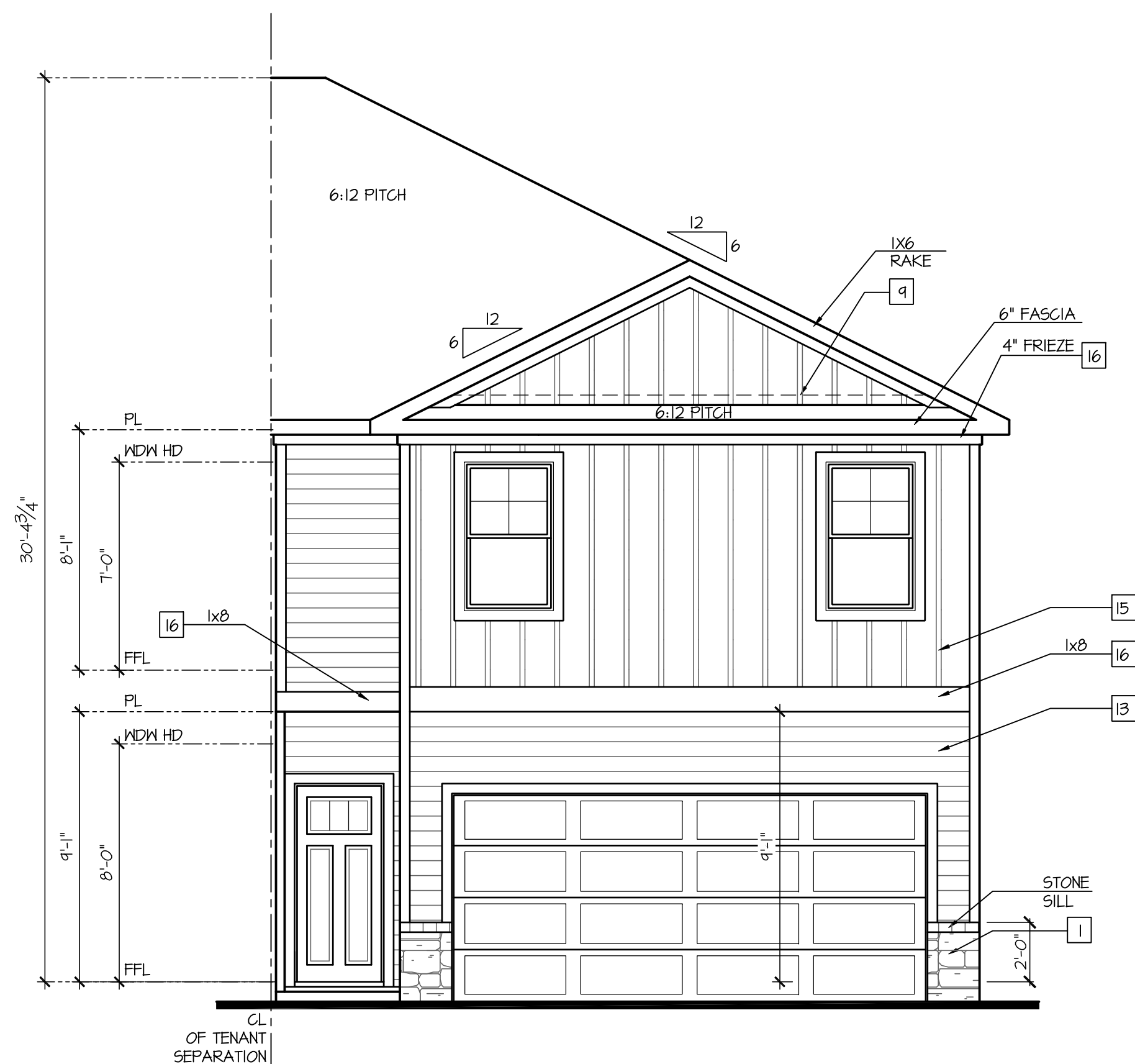
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A6.16



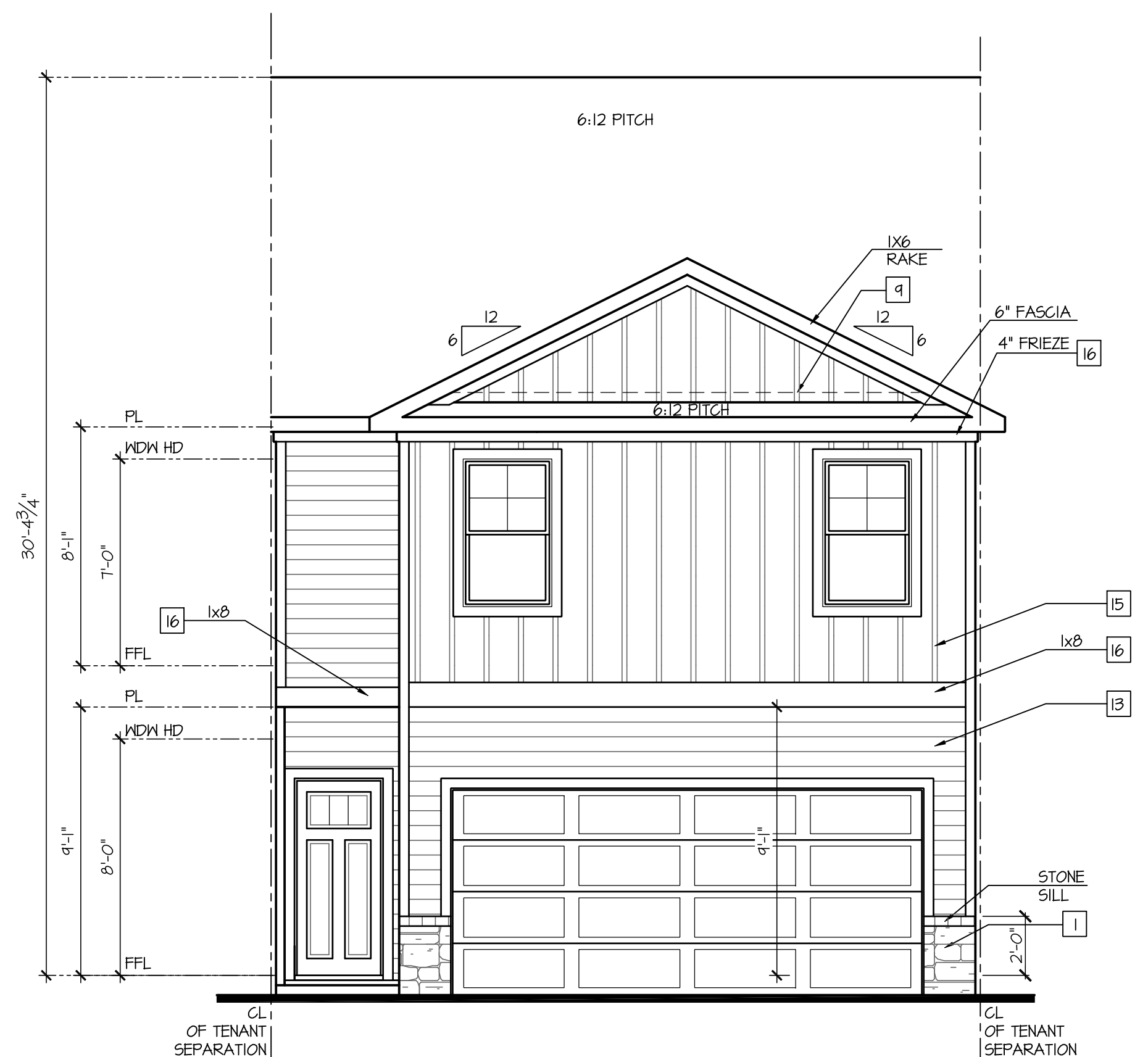
Front Elevation 'A-1'
At End Unit Condition - L

SCALE: 1/4"=1'-0" AT 22'X34" LAYOUT 1/8"=1'-0" AT 11'X17" LAYOUT



Front Elevation 'A-1'
At End Unit Condition - R

SCALE: 1/4"=1'-0" AT 22'X34" LAYOUT 1/8"=1'-0" AT 11'X17" LAYOUT



Front Elevation 'A-1'
At Interior Unit Condition

SCALE: 1/4"=1'-0" AT 22'X34" LAYOUT 1/8"=1'-0" AT 11'X17" LAYOUT

NO.	DATE	REVISION
1	06/22/24	

PROFESSIONAL SEAL:

PROJECT TITLE:

**Townhomes
Master Set
'Pattison'**

CLIENTS NAME:

D-R-HORTON
America's Builder

PROJECT NO: GMD21038.4

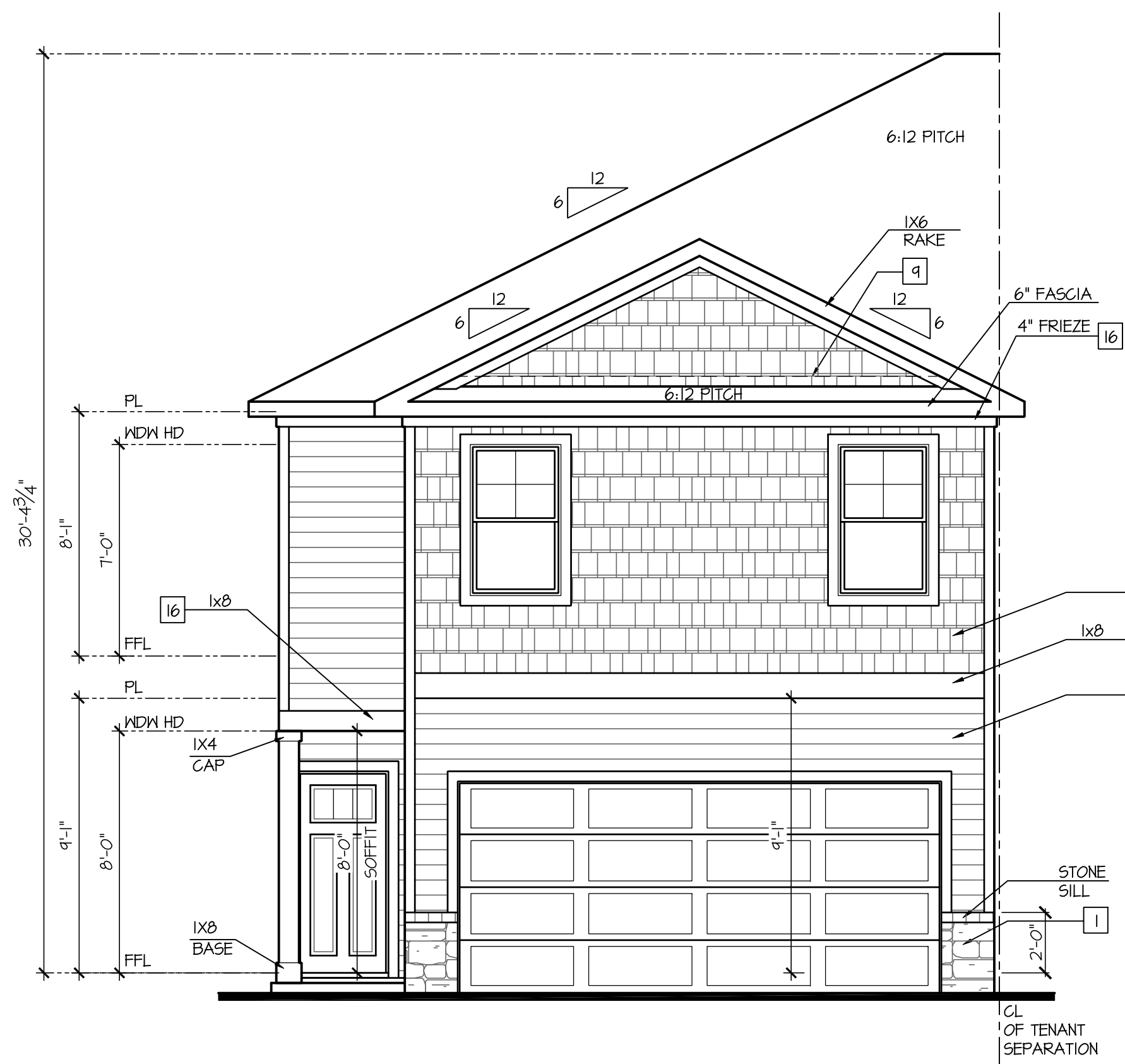
SHEET TITLE:
**SYDNEY
EXTERIOR
ELEVATIONS 'B'**

PRINT DATE:
Oct. 24, 2023

SHEET NO:
A6.0

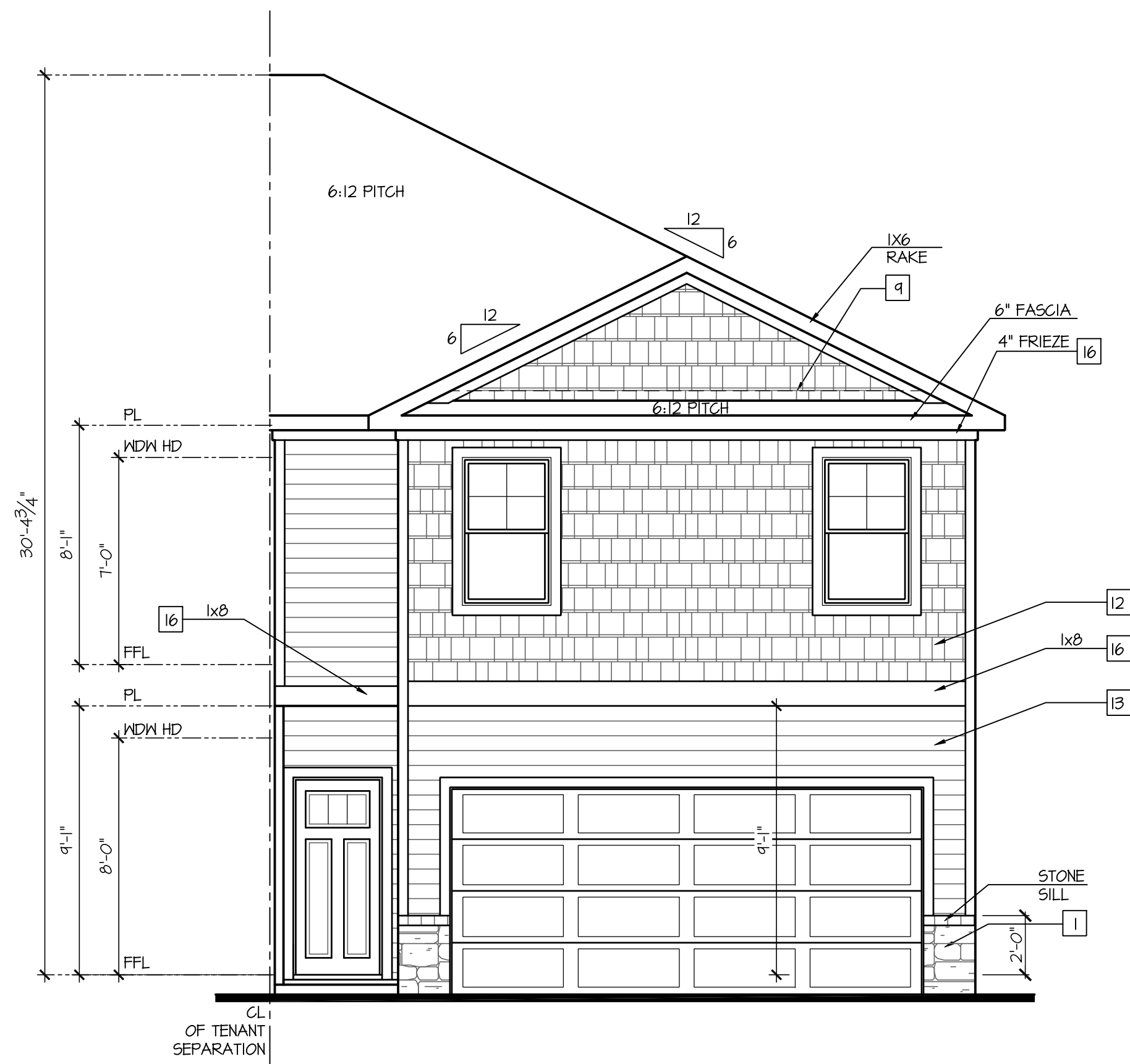
**FOR
CONSTRUCTION**

NOTES:	KEY NOTES:
<ul style="list-style-type: none"> - GRADE CONDITIONS MAY VARY FOR INDIVIDUAL SITE FROM THAT SHOWN. BUILDER SHALL VERIFY AND COORDINATE PER ACTUAL SITE CONDITIONS. - WINDOW HEAD HEIGHTS: 1ST FLOOR = 7'-0" U.N.O. ON ELEVATIONS. 2ND FLOOR = 7'-0" U.N.O. ON ELEVATIONS. - ROOFING: PITCHED SHINGLES PER DEVELOPER. - WINDOWS: MANUFACTURER PER DEVELOPER. DIVIDED LITES AS SHOWN ON THE EXTERIOR ELEVATIONS - ENTRY DOOR: AS SELECTED BY DEVELOPER. - GARAGE DOORS: AS SELECTED BY DEVELOPER, RAISED PANEL AS SHOWN. - CHIMNEY AS OCCURS: TOP OF CHIMNEYS TO BE A MINIMUM OF 24" ABOVE ANY ROOF WITHIN 10'-0" OF CHIMNEY. - ALL EXTERIOR MATERIALS TO BE INSTALLED PER MANUFACTURER'S WRITTEN INSTRUCTIONS. - PROTECTION AGAINST DECAY: (ALL PORTIONS OF A PORCH, SCREEN PORCH OR DECK FROM THE BOTTOM OF THE HEADER DOWN, INCLUDING POST, RAILS, PICKETS, STEPS AND FLOOR STRUCTURE) - SHINGLED ROOF PITCHES FROM 2/12 TO 4/12 TO HAVE (2) LAYERS OF UNDERLAYMENT APPLIED AND OVERLAPPED PER R405.2.7. 	<p>MASONRY:</p> <ol style="list-style-type: none"> ADHERED STONE VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED. MASONRY FULL BRICK AS SELECTED BY DEVELOPER. HEIGHT AS NOTED. ADHERED BRICK VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED. 8" SOLDIER COURSE. ROWLOCK COURSE DECORATIVE KEY. SEE DETAIL. <p>TYPICALS:</p> <ol style="list-style-type: none"> CORROSION RESISTANT SCREEN LOWERED VENTS, SIZE AS NOTED. CODE APPROVED TERMINATION CHIMNEY CAP. CORROSION RESISTANT ROOF TO WALL FLASHING. CODE COMPLIANT FLASHING MUST BE INSTALLED AT ALL ROOF/WALL INTERSECTIONS. STANDING SEAM METAL ROOF, INSTALL PER MANUFACTURER'S WRITTEN INSTRUCTIONS. ROOFING FINISH, PER SPECS <p>SIDING:</p> <ol style="list-style-type: none"> SHAKE SIDING PER DEVELOPER LAP SIDING PER DEVELOPER N/A PANEL SIDING W/ 1X3 BATTS AT 12" O.C. (BATT SIDING) 1X TRIM OR EQUAL, U.N.O. SIZE AS NOTED SHUTTERS, TYPE AS SHOWN. SIZE AS NOTED. <p>ALL WINDOWS WHOSE OPENING IS LESS THAN 24" ABOVE THE FINISH FLOOR AND WHOSE OPENING IS GREATER THAN 12" ABOVE THE OUTSIDE WALKING SURFACE MUST HAVE WINDOW OPENING LIMITING DEVICES COMPLYING WITH LOCAL CODES</p>



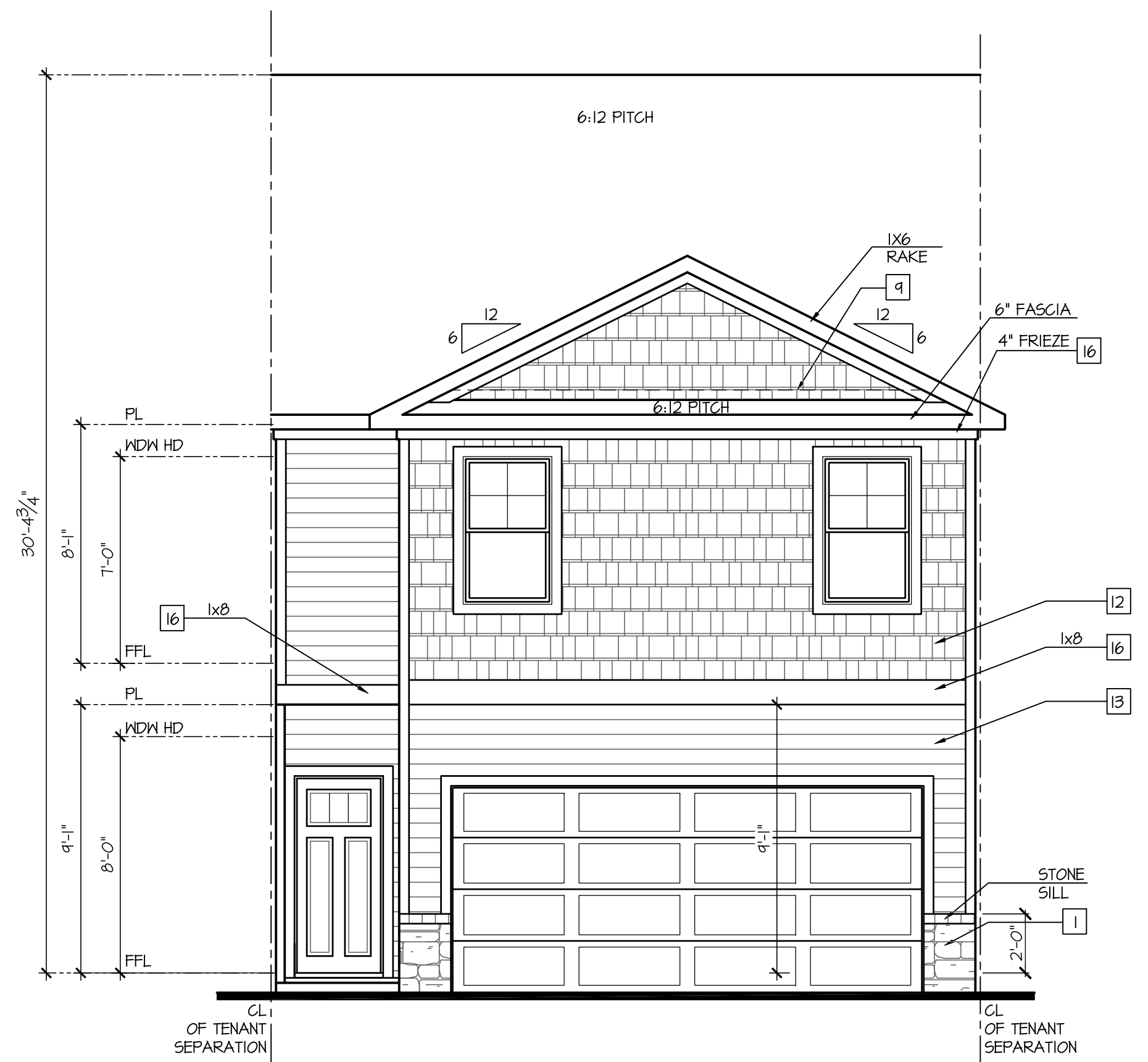
Front Elevation 'A-2'
At End Unit Condition - L

SCALE: 1/4"=1'-0" AT 22'X34" LAYOUT 1/8"=1'-0" AT 11'X11" LAYOUT



Front Elevation 'A-2'
At End Unit Condition - R

SCALE: 1/4"=1'-0" AT 22'X34" LAYOUT 1/8"=1'-0" AT 11'X11" LAYOUT



Front Elevation 'A-2'
At Interior Unit Condition

SCALE: 1/4"=1'-0" AT 22'X34" LAYOUT 1/8"=1'-0" AT 11'X11" LAYOUT

NO.	DATE	REVISION
1	06/22/24	

PROFESSIONAL SEAL:

PROJECT TITLE:

**Townhomes
Master Set
'Pattison'**

CLIENTS NAME:

D-R-HORTON
America's Builder

PROJECT NO: GMD21038.4

SHEET TITLE:
**SYDNEY
EXTERIOR
ELEVATIONS 'B'**

PRINT DATE:
Oct. 24, 2023

SHEET NO:
A6.1

**FOR
CONSTRUCTION**

NOTES:	KEY NOTES:
<ul style="list-style-type: none"> - GRADE CONDITIONS MAY VARY FOR INDIVIDUAL SITE FROM THAT SHOWN. BUILDER SHALL VERIFY AND COORDINATE PER ACTUAL SITE CONDITIONS. - WINDOW HEAD HEIGHTS: 1ST FLOOR = 7'-0" U.N.O. ON ELEVATIONS. 2ND FLOOR = 7'-0" U.N.O. ON ELEVATIONS. - ROOFING: PITCHED SHINGLES PER DEVELOPER. - WINDOWS: MANUFACTURER PER DEVELOPER. DIVIDED LITES AS SHOWN ON THE EXTERIOR ELEVATIONS - ENTRY DOOR: AS SELECTED BY DEVELOPER. - GARAGE DOORS: AS SELECTED BY DEVELOPER, RAISED PANEL AS SHOWN. - CHIMNEY AS OCCURS: TOP OF CHIMNEYS TO BE A MINIMUM OF 24" ABOVE ANY ROOF WITHIN 10'-0" OF CHIMNEY. - ALL EXTERIOR MATERIALS TO BE INSTALLED PER MANUFACTURER'S WRITTEN INSTRUCTIONS. - PROTECTION AGAINST DECAY: (ALL PORTIONS OF A PORCH, SCREEN PORCH OR DECK FROM THE BOTTOM OF THE HEADER DOWN, INCLUDING POST, RAILS, PICKETS, STEPS AND FLOOR STRUCTURE) - SHINGLED ROOF PITCHES FROM 2/12 TO 4/12 TO HAVE (2) LAYERS OF UNDERLAYMENT APPLIED AND OVERLAPPED PER R405.2.1. 	<p>MASONRY:</p> <ol style="list-style-type: none"> ADHERED STONE VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED. MASONRY FULL BRICK AS SELECTED BY DEVELOPER. HEIGHT AS NOTED. ADHERED BRICK VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED. 8" SOLDIER COURSE. ROWLOCK COURSE DECORATIVE KEY. SEE DETAIL. <p>TYPICALS:</p> <ol style="list-style-type: none"> CORROSION RESISTANT SCREEN LOWERED VENTS, SIZE AS NOTED. CODE APPROVED TERMINATION CHIMNEY CAP. CORROSION RESISTANT ROOF TO WALL FLASHING. CODE COMPLIANT FLASHING MUST BE INSTALLED AT ALL ROOF/WALL INTERSECTIONS. STANDING SEAM METAL ROOF, INSTALL PER MANUFACTURER'S WRITTEN INSTRUCTIONS. ROOFING FINISH, PER SPECS <p>SIDING:</p> <ol style="list-style-type: none"> SHAKE SIDING PER DEVELOPER LAP SIDING PER DEVELOPER N/A PANEL SIDING W/ 1X3 BATTS AT 12" O.C. (BATT SIDING) 1X TRIM OR EQUAL, U.N.O. SIZE AS NOTED SHUTTERS, TYPE AS SHOWN. SIZE AS NOTED. <p>ALL WINDOWS WHOSE OPENING IS LESS THAN 24" ABOVE THE FINISH FLOOR AND WHOSE OPENING IS GREATER THAN 12" ABOVE THE OUTSIDE WALKING SURFACE MUST HAVE WINDOW OPENING LIMITING DEVICES COMPLYING WITH LOCAL CODES</p>

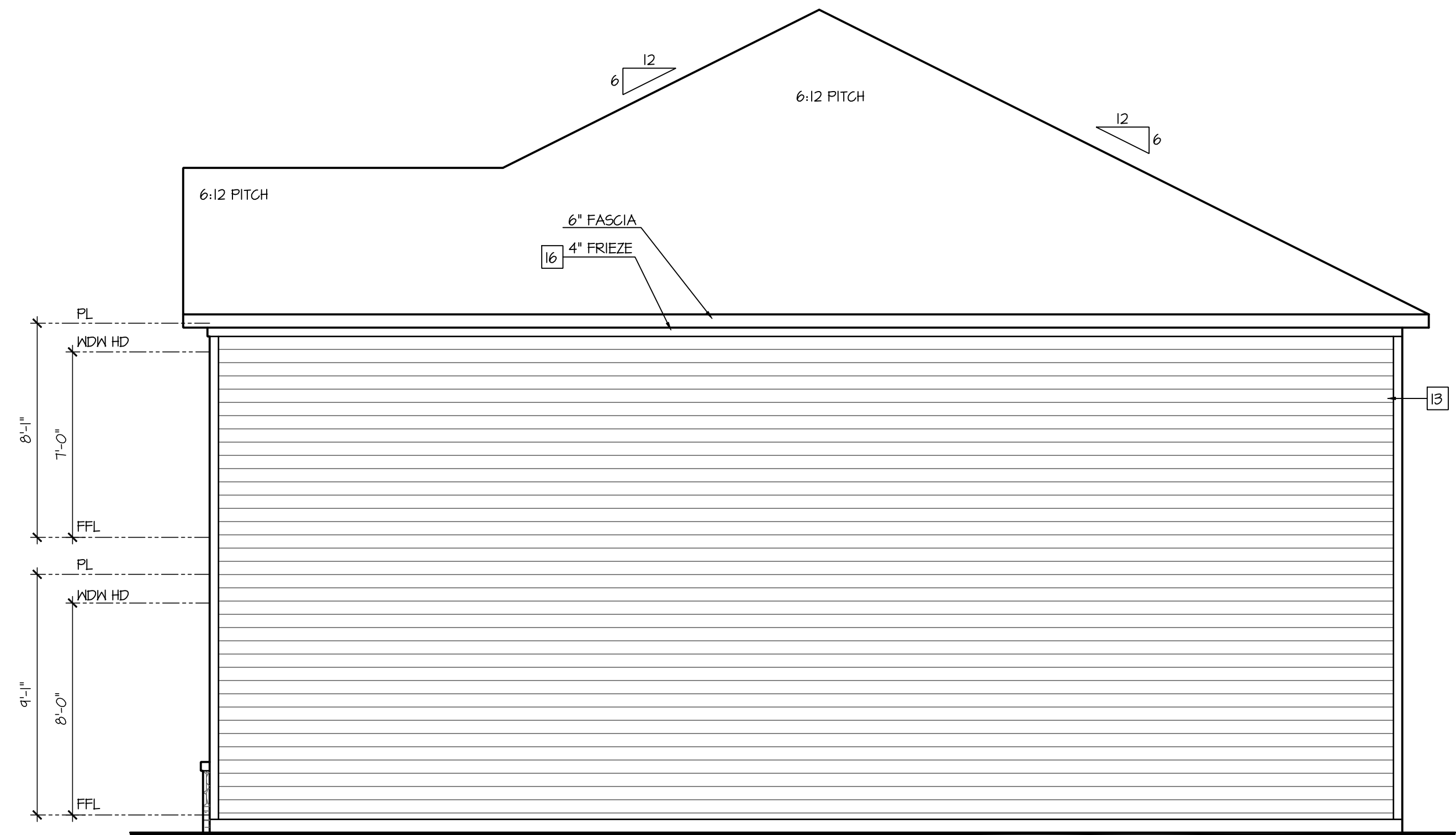
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1ST FLOOR = 7'-0" U.N.O. ON ELEVATIONS.
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- WINDOWS: MANUFACTURER PER DEVELOPER. DIVIDED LITES AS SHOWN ON THE EXTERIOR ELEVATIONS
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- PROTECTION AGAINST DECAY:
(ALL PORTIONS OF A PORCH, SCREEN PORCH OR DECK FROM THE BOTTOM OF THE HEADER DOWN, INCLUDING POST, RAILS, PICKETS, STEPS AND FLOOR STRUCTURE)

KEY NOTES:

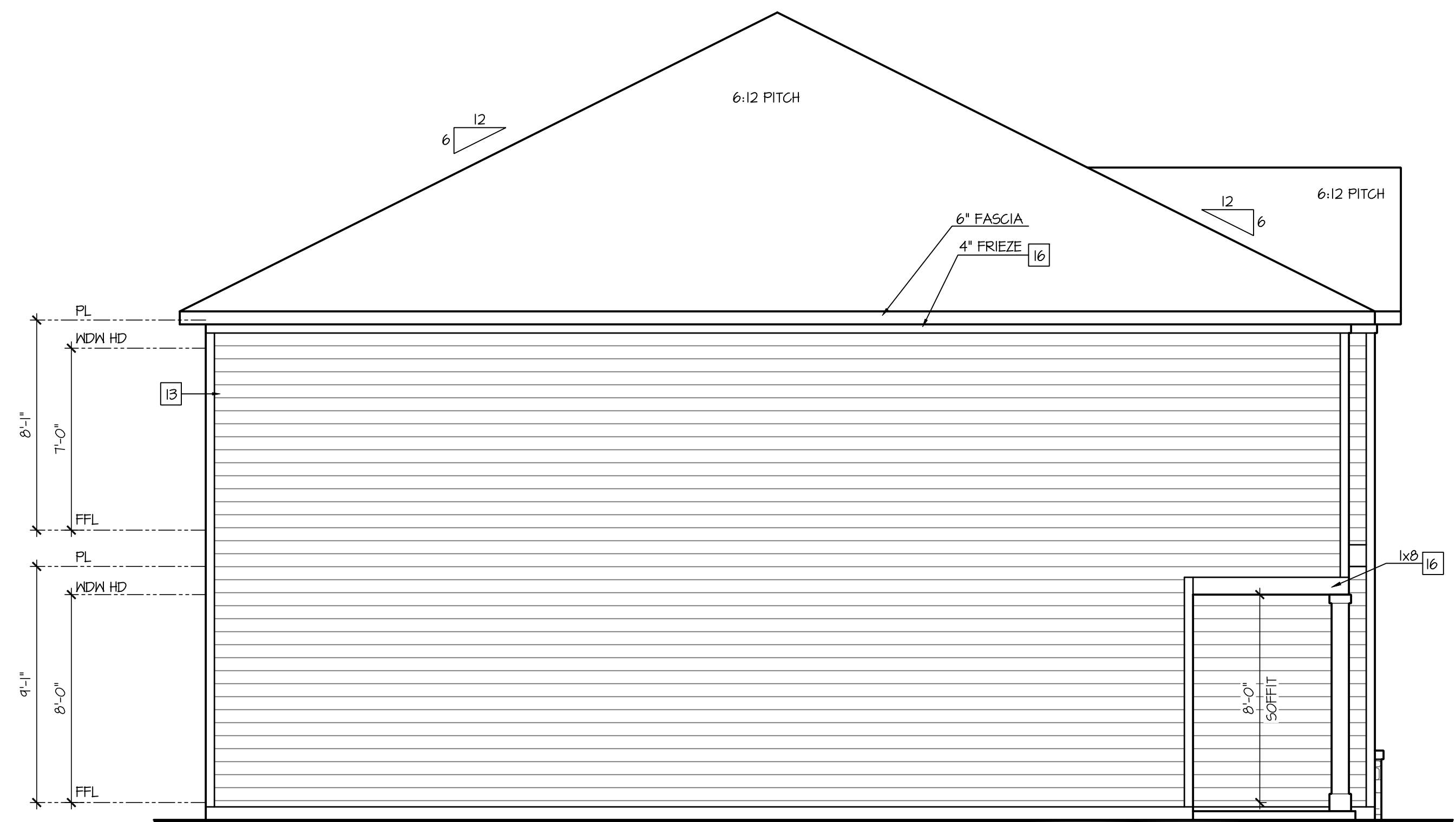
- MASONRY:**
- [1] ADHERED STONE VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.
 - [2] MASONRY FULL BRICK AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.
 - [3] ADHERED BRICK VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.
 - [4] 8" SOLDIER COURSE.
 - [5] RIBLOCK COURSE
 - [6] DECORATIVE KEY. SEE DETAIL.
- TYPICALS:**
- [7] CORROSION RESISTANT SCREEN LOUVERED VENTS, SIZE AS NOTED.
 - [8] CODE APPROVED TERMINATION CHIMNEY CAP.
 - [9] CORROSION RESISTANT ROOF TO WALL FLASHING. CODE COMPLIANT FLASHING MUST BE INSTALLED AT ALL ROOF/WALL INTERSECTIONS.
- ROOFING:**
- [10] STANDING SEAM METAL ROOF, INSTALL PER MANUFACTURER'S WRITTEN INSTRUCTIONS.
 - [11] ROOFING FINISH, PER SPECS
- SIDING:**
- [12] SHAKE SIDING PER DEVELOPER
 - [13] LAP SIDING PER DEVELOPER
 - [14] N/A
 - [15] PANEL SIDING W/ 1X3 BATTS AT 12" O.C. (BATT SIDING)
 - [16] 1X TRIM OR EQUAL, U.N.O. SIZE AS NOTED
 - [17] SHUTTERS, TYPE AS SHOWN. SIZE AS NOTED.

ALL WINDOWS WHOSE OPENING IS LESS THAN 24" ABOVE THE FINISH FLOOR AND WHOSE OPENING IS GREATER THAN 12" ABOVE THE OUTSIDE WALKING SURFACE MUST HAVE WINDOW OPENING LIMITING DEVICES COMPLYING WITH LOCAL CODES



Right Elevation 'A-1'/'A-2'
At End Condition - R

SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X17" LAYOUT



Left Elevation 'A-1'/'A-2'
At End Condition - L

SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X17" LAYOUT

NO.	DATE:	REVISION:
1	06/22/24	

PROFESSIONAL SEAL:

PROJECT TITLE:

**Townhomes
Master Set
'Pattison'**

**FOR
CONSTRUCTION**

CLIENTS NAME:

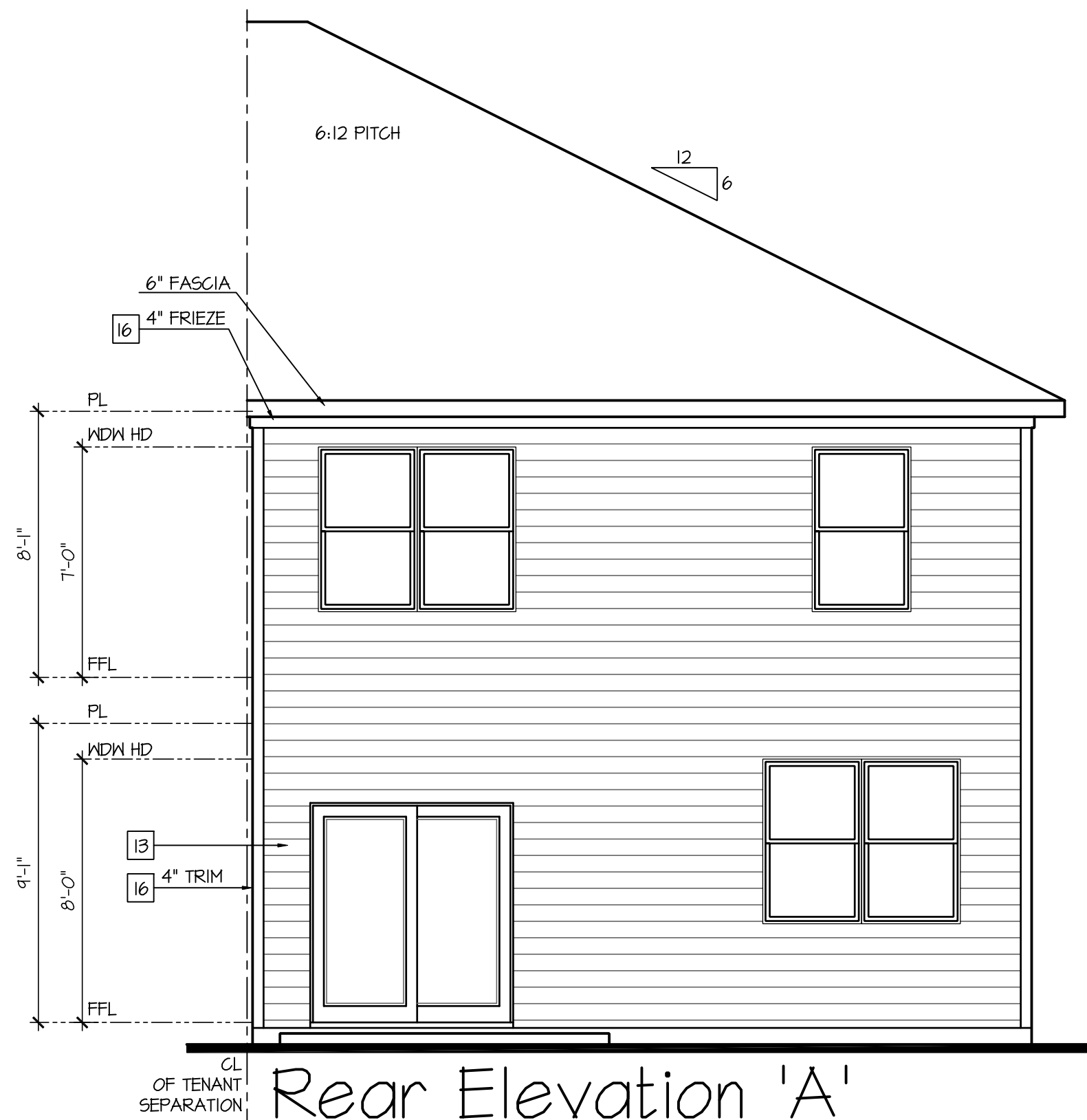
D-R-HORTON
America's Builder

PROJECT NO: GMD21038.4

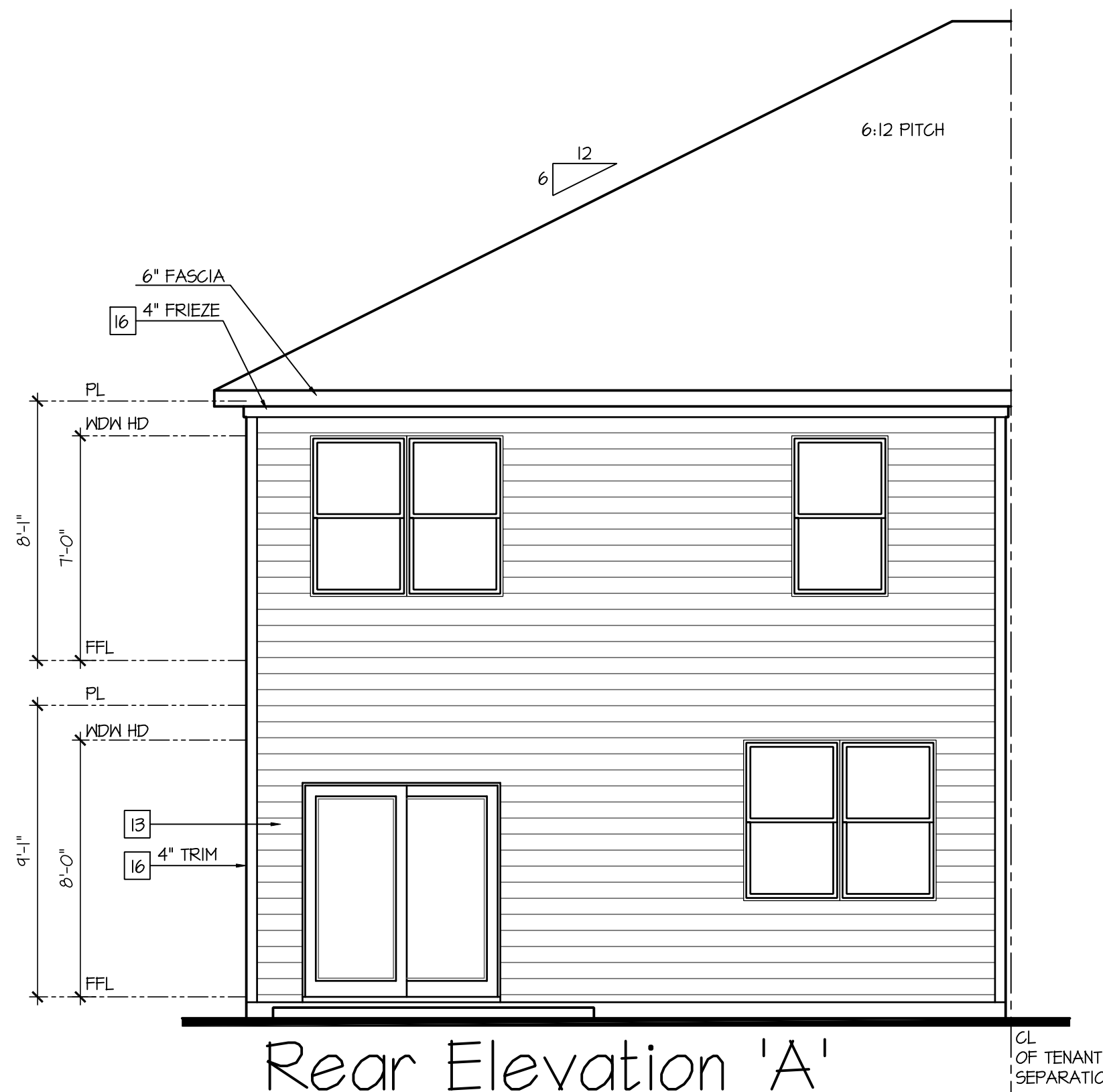
SHEET TITLE:
**SYDNEY
EXTERIOR
ELEVATIONS 'A'**

PRINT DATE:
Oct. 24, 2023

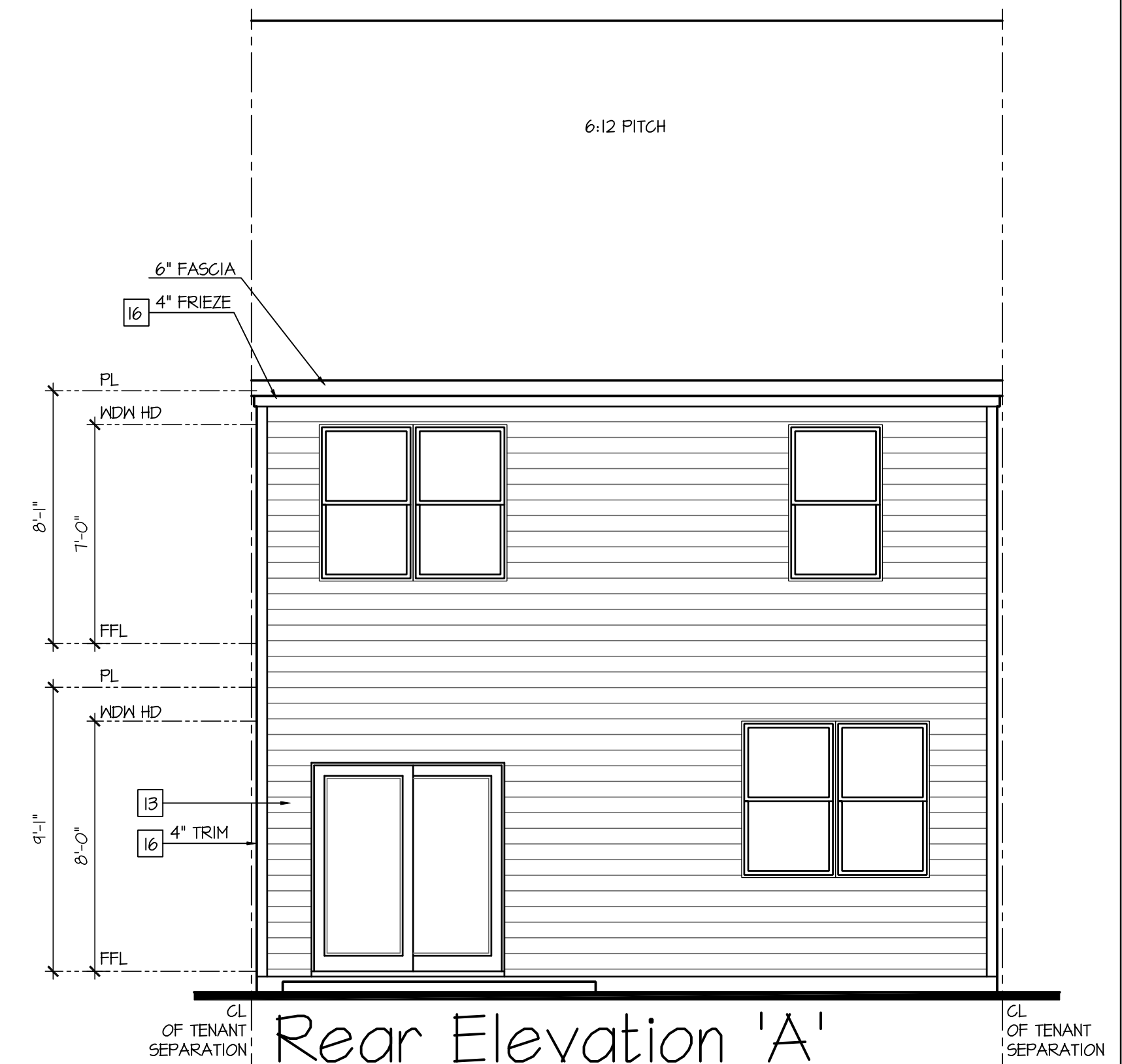
SHEET NO:
A6.4



Rear Elevation 'A'
At End Unit Condition - L
SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X11" LAYOUT
AT ELEVATIONS 'A-1', 'A-2', 'A-3' AND 'A-4'



Rear Elevation 'A'
At End Unit Condition - R
SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X11" LAYOUT
AT ELEVATIONS 'A-1', 'A-2', 'A-3' AND 'A-4'



Rear Elevation 'A'
At Interior Unit Condition
SCALE: 1/4"=1'-0" AT 22"X34" LAYOUT 1/8"=1'-0" AT 11"X11" LAYOUT
AT ELEVATIONS 'A-1', 'A-2', 'A-3' AND 'A-4'

NOTES:

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KEY NOTES:

MASONRY:	SIDING:
1 ADHERED STONE VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.	2 SHAKE SIDING PER DEVELOPER
2 MASONRY FULL BRICK AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.	3 LAP SIDING PER DEVELOPER
3 ADHERED BRICK VENEER AS SELECTED BY DEVELOPER. HEIGHT AS NOTED.	4 N/A
4 8" SOLDIER COURSE.	5 PANEL SIDING W/ 1X3 BATTS AT 12" O.C. (BATT SIDING)
5 ROWLOCK COURSE	6 1X TRIM OR EQUAL, U.N.O. SIZE AS NOTED
6 DECORATIVE KEY. SEE DETAIL.	7 SHUTTERS, TYPE AS SHOWN. SIZE AS NOTED.
TYPICALS:	
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ALL WINDOWS WHOSE OPENING IS LESS THAN 24" ABOVE THE FINISH FLOOR AND WHOSE OPENING IS GREATER THAN 72" ABOVE THE OUTSIDE WALKING SURFACE MUST HAVE WINDOW OPENING LIMITING DEVICES COMPLYING WITH LOCAL CODES

NO:	DATE:	REVISION:
1	06/22/24	

PROFESSIONAL SEAL:

PROJECT TITLE:

**Townhomes
Master Set
'Pattison'**

CLIENTS NAME:

D-R HORTON
America's Builder

PROJECT NO: GMD21038.4

SHEET TITLE:
**SYDNEY
EXTERIOR
ELEVATIONS 'A'**

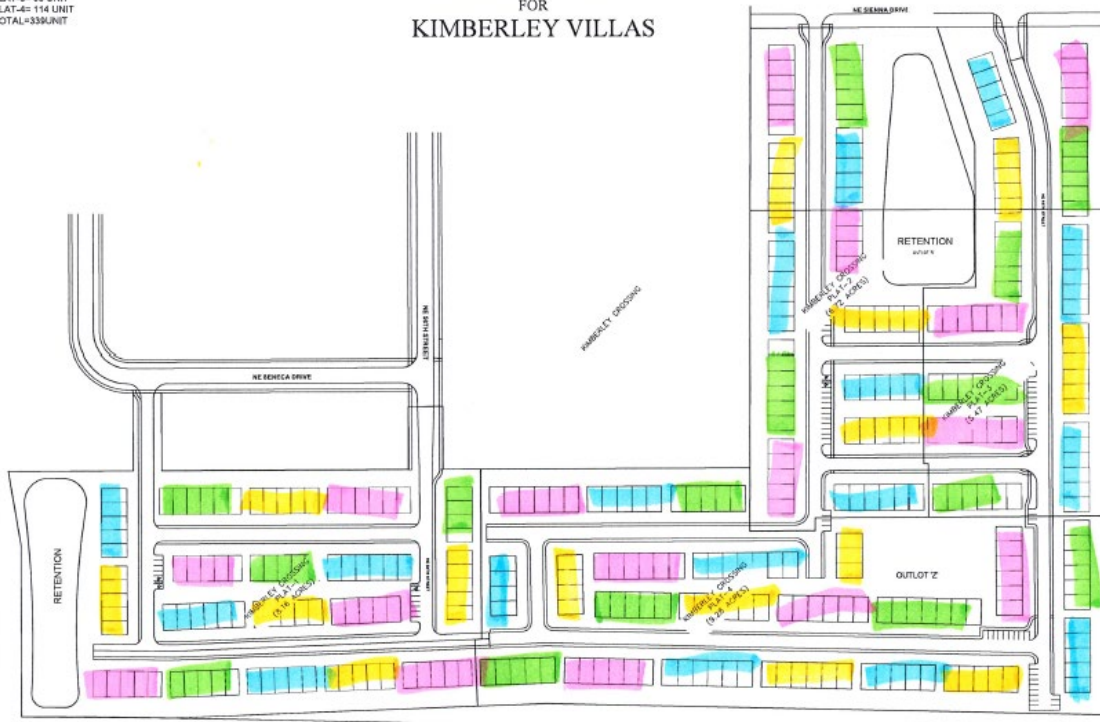
PRINT DATE:
Oct. 24, 2023

SHEET NO:
A6.16

**FOR
CONSTRUCTION**

PLAT-1= 93 UNIT
 PLAT-2= 64 UNIT
 PLAT-3= 68 UNIT
 PLAT-4= 114 UNIT
 TOTAL=339UNIT

CONCEPT PLAN FOR KIMBERLEY VILLAS



KIMBERLEY VILLAS
 ANNEX, IA

STUBBS ENGINEERING
 401 1ST ST. SUITE 100, ANNEX, IA 52003





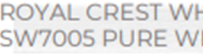





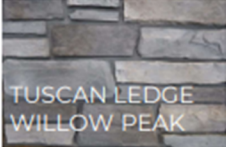









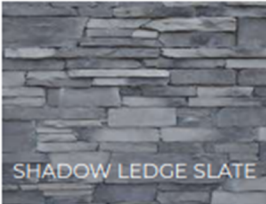
CONCEPT PLAN

NOV 10/23/23
 TIME: PM
 BY: BWS
 PROJECT NO: 23-101
 SHEET NO: 1

-  = Harvest Moon
-  = Royal Crest
-  = Midnight Boardwalk
-  = Coastal Highway

Color Schemes: We only have multiple color schemes using these siding colors below.

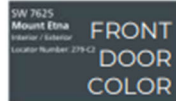
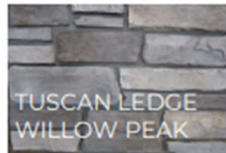
End Units	Board/Batten or Shakes	Inner Units	Board/Batten or Shakes
 <p>ROYAL CREST HARVARD SLATE / SW7018 DOVETAIL</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>ROYAL CREST HARVARD SLATE / SW7018 DOVETAIL</p>
 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>FRONT DOOR COLOR</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>FRONT DOOR COLOR</p>
 <p>TUSCAN LEDGE WILLOW PEAK</p>	 <p>FRONT DOOR COLOR</p>	 <p>TUSCAN LEDGE WILLOW PEAK</p>	 <p>FRONT DOOR COLOR</p>
HARVEST MOON			

End Units	Board/Batten or Shakes	Inner Units	Board/Batten or Shakes
 <p>ROYAL CREST STERLING / SW7667 ZIRCON</p>	 <p>ROYAL WEATHERED GRAY</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>
 <p>ROYAL WHITE / SW7005 PURE WHITE</p>	 <p>FRONT DOOR COLOR</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>
 <p>SHADOW LEDGE SLATE</p>	 <p>FRONT DOOR COLOR</p>	 <p>SHADOW LEDGE SLATE</p>	 <p>FRONT DOOR COLOR</p>
ROYAL CREST STERLING			

End Units



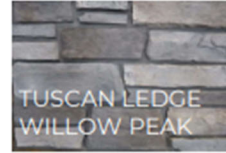
Ranch



Inner Units

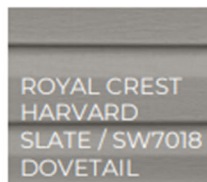


Ranch

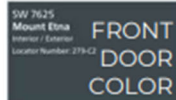
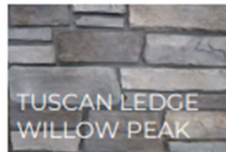


MIDNIGHT BOARDWALK

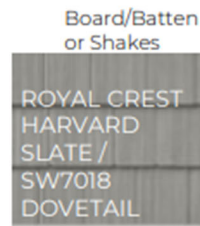
End Units



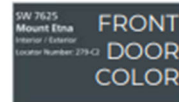
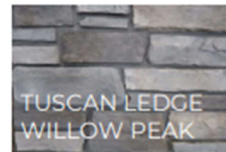
Ranch



Inner Units



Ranch



COASTAL HIGHWAY

Kimberley Crossing Villas Submittal:

Pattison Plan (Plat 1- approx. 93 units):

Public Road Facing Units



front elevation



Pattison Rear View Hi-Viz



Pattison Left Hi-Viz



Pattison Right Hi-Viz

Pattison Plan Public Facing Road

Front		
Material	Quantity Square Ft.	Percentage of Total
Lap	177	36%
B&B	105	21%
Shake	156	31%
Masonry	30	6%
Glazing	30	6%
Entry/Garage Doors	0	0%
Total	498	

Rear End Unit (Hi-Viz)		
Material	Quantity Square Ft.	Percentage of Total
Lap	178	43%
B&B		
Shake		
Masonry	128	31%
Glazing	111	27%
Entry/Garage Doors		
Total	417	

Rear Middle Unit (Hi-Viz)		
Material	Quantity Square Ft.	Percentage of Total
Lap	194	47%
B&B		
Shake		
Masonry	128	31%
Glazing	95	23%
Entry/Garage Doors		
Total	417	

End (Hi-Viz) X2		
Material	Quantity Square Ft.	Percentage of Total
Lap	385	51%
B&B		
Shake		
Masonry	310	41%
Glazing	60	8%
Entry/Garage Doors		
Total	755	

Private Road Facing Units:



front elevation



Pattison Rear View



Pattison Left



Pattison Right

Pattison Plan (Private Road Facing Units)

Front		
Material	Quantity Square Ft.	Percentage of Total
Lap	177	36%
B&B	105	21%
Shake	156	31%
Masonry	30	6%
Glazing	30	6%
Entry/Garage Doors	0	0%
Total	498	

Rear End Unit (3'-0" Stone)		
Material	Quantity Square Ft.	Percentage of Total
Lap	235	56%
B&B		
Shake		
Masonry	72	17%
Glazing	111	27%
Entry/Garage Doors		
Total	418	

Rear Middle Unit (3'-0" Stone)		
Material	Quantity Square Ft.	Percentage of Total
Lap	250	60%
B&B		
Shake		
Masonry	72	17%
Glazing	96	23%
Entry/Garage Doors		
Total	418	

End (3'-0" Stone) X2			
	Material	Quantity Square Ft.	Percentage of Total
	Lap	592	75%
	B&B		
	Shake		
	Masonry	135	17%
	Glazing	60	8%
	Entry/Garage Doors		
	Total	787	

Sydney Plan (Plat 2-4, approx. 224 units):

Public Road Facing Units:



Front elevation



Rear elevation



Left elevation



Right elevation

Front			End (Hi-Viz) Right		
Material	Quantity Square Ft.	Percentage of Total	Material	Quantity Square Ft.	Percentage of Total
Lap	288	55%	Lap	302	47%
B&B	130	25%	B&B		
Shake			Shake		
Masonry	67	13%	Masonry	344	53%
Glazing	42	8%	Glazing		
Entry/Garage Doors			Entry/Garage Doors		
Total	527		Total	646	

Rear End Unit (Hi-Viz)		
Material	Quantity Square Ft.	Percentage of Total
Lap	178	37%
B&B		
Shake		
Masonry	244	51%
Glazing	96	20%
Entry/Garage Doors		
Total	482	

End (Hi-Viz) Left		
Material	Quantity Square Ft.	Percentage of Total
Lap	303	50%
B&B		
Shake		
Masonry	307	50%
Glazing		
Entry/Garage Doors		
Total	610	

Rear Middle Unit (Hi-Viz)		
Material	Quantity Square Ft.	Percentage of Total
Lap	178	32%
B&B		
Shake		
Masonry	275	50%
Glazing	96	17%
Entry/Garage Doors		
Total	549	

Private Road Facing Units:



Front elevation



Rear Elevation



Left Elevation



Right Elevation

Sydney Plan Private Road Facing











Front			Rear End Unit (3'-0" Stone)		
Material	Quantity Square Ft.	Percentage of Total	Material	Quantity Square Ft.	Percentage of Total
Lap	288	55%	Lap	309	64%
B&B	130	25%	B&B		
Shake			Shake		
Masonry	67	13%	Masonry	87	18%
Glazing	42	8%	Glazing	96	20%
Entry/Garage Doors			Entry/Garage Doors		
Total	527		Total	482	












Rear Middle Unit (3'-0" Stone)		
Material	Quantity Square Ft.	Percentage of Total
Lap	319	62%
B&B		
Shake		
Masonry	99	19%
Glazing	96	19%
Entry/Garage Doors		
Total	514	

End Left (3'-0" Stone)		
Material	Quantity Square Ft.	Percentage of Total
Lap	592	81%
B&B		
Shake		
Masonry	135	19%
Glazing		
Entry/Garage Doors		
Total	727	

End Right (3'-0" Stone)		
Material	Quantity Square Ft.	Percentage of Total
Lap	581	81%
B&B		
Shake		
Masonry	135	19%
Glazing		
Entry/Garage Doors		
Total	716	

Color Schemes: We only have multiple color schemes using these siding colors below.

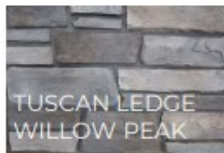
End Units	Board/Batten or Shakes	Inner Units	Board/Batten or Shakes
 <p>ROYAL CREST HARVARD SLATE / SW7018 DOVETAIL</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>ROYAL CREST HARVARD SLATE / SW7018 DOVETAIL</p>
 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>SW 7625 Mount Etna Interior / Exterior Locator Number: 279-C2</p> <p>FRONT DOOR COLOR</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>SW 7625 Mount Etna Interior / Exterior Locator Number: 279-C2</p> <p>FRONT DOOR COLOR</p>
 <p>TUSCAN LEDGE WILLOW PEAK</p>		 <p>TUSCAN LEDGE WILLOW PEAK</p>	
HARVEST MOON			

End Units	Board/Batten or Shakes	Inner Units	Board/Batten or Shakes
 <p>ROYAL CREST STERLING / SW7667 ZIRCON</p>	 <p>ROYAL WEATHERED GRAY</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>
 <p>ROYAL WHITE / SW7005 PURE WHITE</p>	 <p>SW 7625 Mount Etna Interior / Exterior Locator Number: 279-C2</p> <p>FRONT DOOR COLOR</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>	 <p>ROYAL CREST WHITE / SW7005 PURE WHITE</p>
 <p>SHADOW LEDGE SLATE</p>		 <p>SHADOW LEDGE SLATE</p>	 <p>SW 7625 Mount Etna Interior / Exterior Locator Number: 279-C2</p> <p>FRONT DOOR COLOR</p>
ROYAL CREST STERLING			

End Units



Ranch



Inner Units

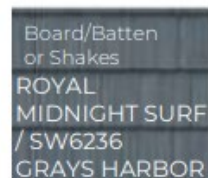


Ranch



MIDNIGHT BOARDWALK

End Units



Ranch



Inner Units



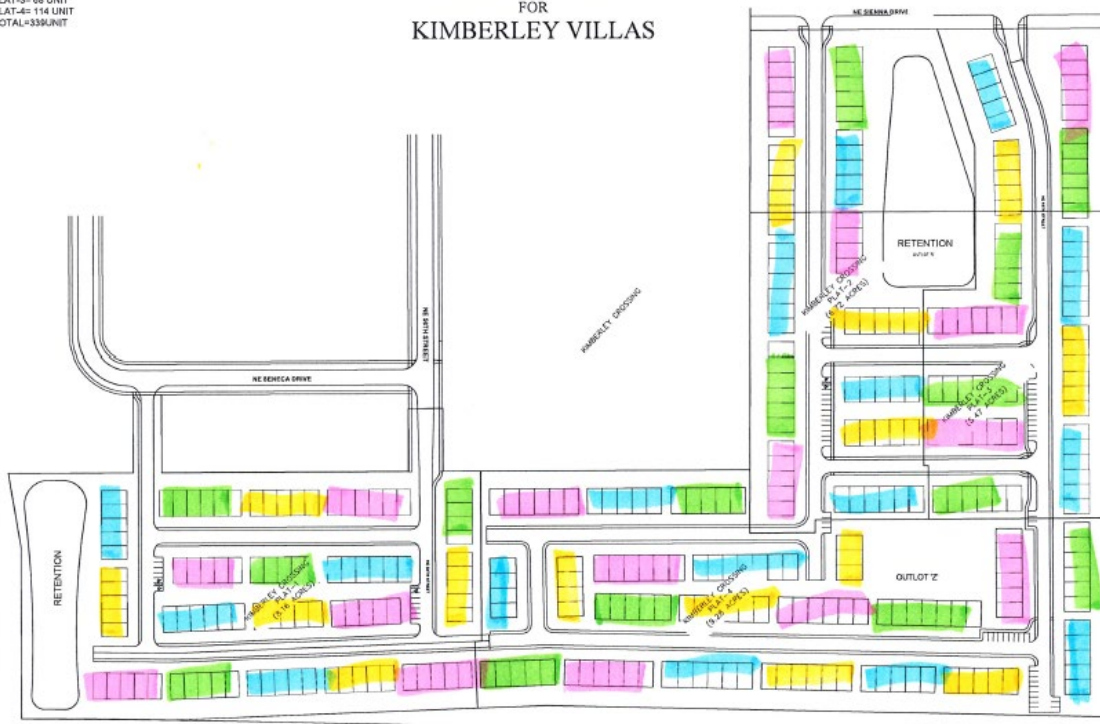
Ranch



COASTAL HIGHWAY

PLAT-1= 93 UNIT
 PLAT-2= 64 UNIT
 PLAT-3= 68 UNIT
 PLAT-4= 114 UNIT
 TOTAL=339UNIT

CONCEPT PLAN FOR KIMBERLEY VILLAS



KIMBERLEY VILLAS
 ANNEX, IA

STUBBS ENGINEERING
 401 1ST ST. SUITE 100, ANNEX, IA 52003
 563.527.1000



CONCEPT PLAN

NOV 10/23/23
 TIME: PM
 BY: BWS
 PROJECT NO: 23-101
 DRAWING NO: 1

- = Harvest Moon
- = Royal Crest
- = Midnight Boardwalk
- = Coastal Highway

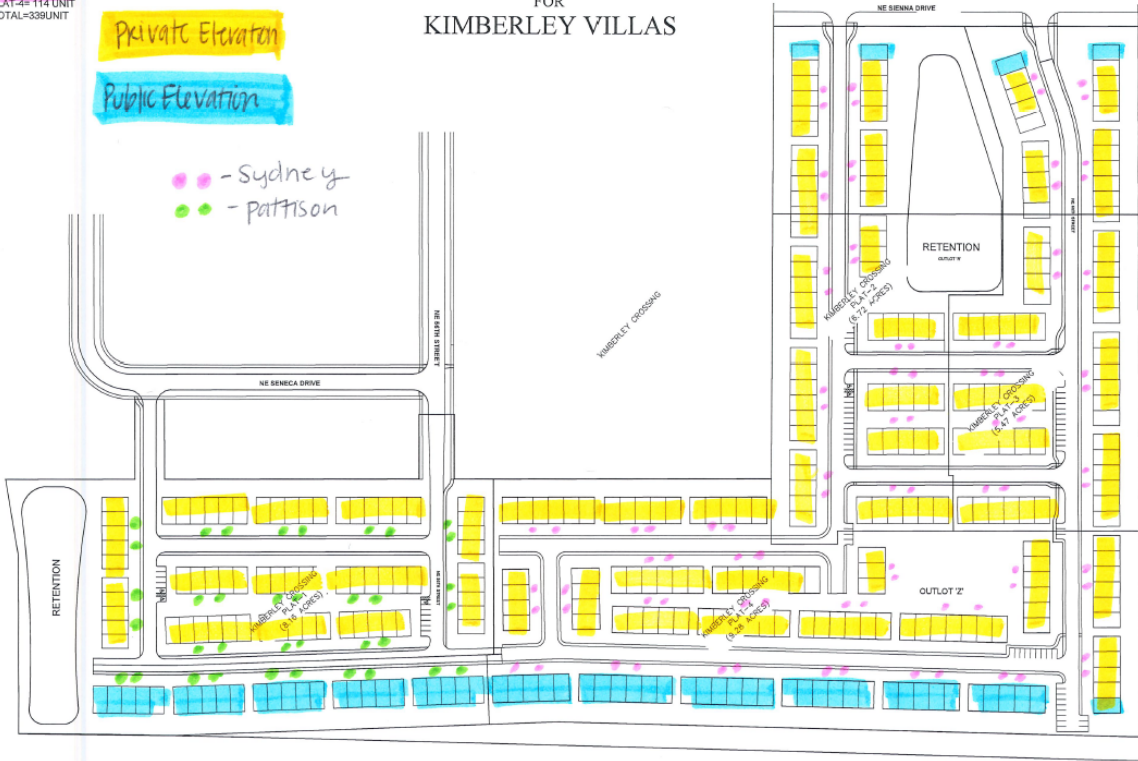
PLAT-1= 93 UNIT
 PLAT-2= 64 UNIT
 PLAT-3= 68 UNIT
 PLAT-4= 114 UNIT
 TOTAL=339UNIT

CONCEPT PLAN FOR KIMBERLEY VILLAS

Private Elevation

Public Elevation

- - Sydney
- - pattison



KIMBERLEY VILLAS
 ANKENY, IA

STUBBS ENGINEERING
 401 W. MAIN STREET, SUITE 100
 DES MOINES, IA 50319



CONCEPT PLAN

DATE: 10/23/23
 DRAWN BY: FN
 CHECKED BY: BWS
 PROJECT NUMBER: 23-191
 TOTAL SHEETS: 1



PLAN AND ZONING COMMISSION
September 17, 2024
6 : 30 PM

Print

ORIGINATING DEPARTMENT:
Planning and Building

COUNCIL GOAL:
Enhance Quality of Life

ACTION REQUESTED:
Motion

LEGAL:
No Review Required

SUBJECT:
4703 N Ankeny Boulevard - Ankeny Community Baptist Church Parking Expansion Site Plan

EXECUTIVE SUMMARY:
See attached staff report.

FISCAL IMPACT: **No**

STAFF RECOMMENDATIONS:





PREVIOUS COUNCIL/COMMISSION/BOARD ACTION(S):

PUBLIC OUTREACH EFFORTS:

ACTION REQUESTED:
Action: Consider MOTION to approve the site plan for 4703 N Ankeny Boulevard, Ankeny Community Baptist Church Parking Expansion.

ADDITIONAL INFORMATION:

ATTACHMENTS:

Click to download
 Aerial Map
 Staff Report
 Applicant Letter
 Site Plan



N



1 inch = 618 feet

Date: 9/11/2024

4703 N Ankeny Boulevard
Ankeny Community Baptist Church Parking Expansion
Aerial Map





Plan and Zoning Commission
Staff Report

Meeting Date: September 17, 2024

Agenda Item: 4703 N Ankeny Boulevard – Ankeny Community Baptist Church Parking Expansion
Report Date: September 10, 2024 *ESC*
Prepared by: Ryan Kirschman
Planner I

Staff Recommendation:

That the Plan and Zoning Commission approve the site plan for 4703 N Ankeny Boulevard, Ankeny Community Baptist Church Parking Expansion.

Project Summary:

The property at 4703 N Ankeny Boulevard is located on Lot 1 Community Baptist Church situated on the northeast corner of the N Ankeny Boulevard and NE 47th Street intersection. The property is zoned PUD, Avondale Trace PUD. Properties to the south and east of the site are similarly zoned PUD, Avondale Trace PUD, properties to the north of the site are zoned R-3, Multiple-Family Residence District, and properties to the west are unincorporated. The future land use for the property is Medium Density Residential per The Ankeny Plan 2040 Figure 12.10 Ankeny Future Land Use Map. The existing building and use align with the zoning and future land use designation. The proposed parking lot expansion is 30,054 sq. ft. This portion of the site is currently open space. The site will add 80 stalls bringing the lot total to 190 spaces. The site will be accessed by two existing access points, one from the east via NE Oak Drive and the other from the south via NE 47th Street. The internal circulation patterns will not increase congestion or decrease safety on the site. A traffic study was not required for the proposed parking addition.

The site has adequate landscaping being added to account for shading and open space requirements. There are 16 trees being added for shading purposes in addition to maintaining the existing trees and shrubs on the site. The proposed addition will utilize the existing screening to the west and south of the proposed parking expansion. This will minimize the impact on existing residential properties to the south. Storm water on the site will be addressed with the existing stormwater retention basin on the southeast portion of the site.

Site Plan Worksheet

4703 N Ankeny Boulevard – Ankeny Community Baptist Church Parking Expansion

The site plan regulations in the zoning code have a variety of design standards and factors for review. The following worksheet is intended to provide staff's comments related to this project and those items.

A. Site Development

- 1. The orientation, alignment, spacing, and placement of a building, driveway, parking area and/or service area on the site shall be compatible with and complimentary to buildings and sites in the neighboring area.**

The site is located at 4703 N Ankeny Boulevard, the proposed location on the site is currently open space.

- 2. The site shall be planned to be compatible with the streetscape that is prevalent in the area.**

The project is compatible with the surrounding streetscape.

- 3. The site shall provide for adequate parking and circulation for vehicles, bicycles and pedestrians. This includes safe bicycle and pedestrian movement from public walks to the building and from parking areas to the building.**

The site will have sufficient parking stalls. The proposed parking lot expansion adds 80 stalls to the parking lot bringing it to a total of 190 stalls. The access to the building for pedestrian and bicycle traffic will remain the same.

- 4. The site shall have such entrances and exits upon adjacent streets and such internal traffic circulation pattern as will not unduly increase congestion or decrease safety on the site or surrounding public streets. Studies of the traffic impact shall be provided if deemed necessary by the Community Development Director.**

The site will be accessed by two existing access points, one from the east via NE Oak Drive and the other from the south via NE 47th Street. The internal circulation patterns will not increase congestion or decrease safety on the site. A traffic study was not required for the proposed parking addition.

- 5. Parking areas shall be treated with decorative elements, building wall extensions, plantings, berms, or other means so as to reduce their impact on public ways and adjoining properties.**

The site has adequate landscaping being added to account for shading and open space requirements. There are 16 trees being added for shading purposes in addition to maintaining the existing trees and shrubs on the site. The proposed addition will utilize the existing screening to the west and south of the proposed parking expansion. This will minimize the impact on existing residential properties to the south.

6. **Service areas; loading and unloading docks, delivery areas, dumpsters, outside storage areas and large storm water detention basins shall be treated with decorative elements, building wall extensions, plantings, berms, or other means so as to screen from view from public ways and adjoining properties.**

The project is screened from the public right of way with the existing shrubs and trees to the west along N Ankeny Boulevard and to the south along NE 47th Street. There will be no additional screening required for this proposal.

7. **All newly installed utility lines shall be underground and entry fixtures located away from high use areas or screened in an approved manner.**

There will be no additional utility lines added to the site.

8. **Exterior lighting, when used, shall enhance the building design and the adjoining landscape. All lighting should be appropriate to the use of the building and surrounding properties with intensity of illumination limited to its intended use and not as an attraction to the site. Lighting shall be directed to eliminate impacts on adjoining sites.**

Any site lighting will be directed away from adjoining property and downward.

9. **The design shall provide adequate provisions for surface and subsurface drainage. Storm water detention, drainage and storm sewer improvements shall be designed to reduce the danger of erosion, flooding, landslide or other endangerment of surrounding property.**

Storm water on the site will be addressed with the existing stormwater retention basin on the southeast portion of the site.

10. **Utility connections to water and sanitary sewer lines shall be designed so as to not overload existing public utility lines. Studies of system loading shall be provided if deemed necessary by the Community Development Director.**

The proposed addition will not be requiring any additional utility connections.

11. **Site design should provide open space in areas visible to the public. A majority of the required open space should be located in front and side yards.**

The overall site layout provides for adequate open space.

12. **Landscaping shall enhance architectural features and contribute to the beauty and utility of a development. Existing trees should be protected whenever possible to maintain the maturity of the site.**

The proposed parking lot shading and open space landscaping conform to the landscape requirements of the Code. The existing site has a few mature trees that will be preserved and relocated.

B. Factors for Evaluation

The following factors and characteristics, which affect the function and appearance of a development, should govern the Plan and Zoning Commission's evaluation of a site plan submission:

1. Conformance to Design Standards and other applicable code requirements.

The plan submitted conforms to the written regulations of the City.

2. Location of the building(s), and the relationship to the development site and neighboring buildings and sites.

The site layout is compatible with setbacks and scale of other sites in the area.

3. Layout and utilization of building, parking, drive-ways, and open spaces.

The site layout is consistent with the surrounding layout.

4. Architectural character, including scale, style, color and type of material, of the building and signage as it relates to the neighborhood.

The proposal on the site will not be adding any architecture.

5. Impact on sanitary sewer, storm sewer, drainage, water, and street systems.

It appears that the impacts on these utility systems can be accommodated adequately.



June 4, 2024

Planning and Zoning Commission and City Council
City of Ankeny
410 W First Street
Ankeny, IA 50023

RE: Community Baptist Church of Ankeny – Parking Lot Expansion

Please accept this letter as a formal request for approval of the parking lot expansion at 4703 N Ankeny Blvd.

Community Baptist Church of Ankeny plans to expand their parking lot. There are two existing accesses to the site. One onto NE 47th Street, one onto NE Oak Drive. An existing detention basin will control the runoff from the site. Landscaping is proposed throughout the site to provide shade and aesthetics to the development.

The expansion will include approximately 30,054 sq. ft. of 6" pavement along with approximately 3,271 sq. ft. of interior landscaped islands. This expansion will increase the number of parking spaces by 80 with 190 spaces provided in total.

We greatly appreciate your consideration of this request.

If you have any further questions or comments, please call.

Sincerely,

A. Leo Pelds Engineering Company

Lance Burke



SITE ADDRESS
4703 N ANKENY BLVD
ANKENY, IA 50021

SITE AREA
9.21 ACRES (401,165 SQ.FT.)

ZONING
EXISTING: PUD - AVONDALE TRACE
ADJACENT ZONING:
NORTH: R-3 MULTI-FAMILY RESIDENTIAL
EAST: PUD - AVONDALE TRACE
SOUTH: PUD
WEST: LOW DENSITY RESIDENTIAL

SETBACKS
PRIMARY FRONTAGE (SOUTH PROPERTY LINE): 40'
NON-PRIMARY FRONTAGE (WEST PROPERTY LINE): 50'
INTERIOR SIDE SETBACK (EAST PROPERTY LINE): 30'
REAR SETBACK (NORTH PROPERTY LINE): 30'

BUILDINGS
EXISTING: IRREGULAR 16,278 SQ. FT.

BUILDING TYPE
COMMERCIAL BUILDING

BUILDING USES
CHURCH

PAVING
APPROACHES: 8" THICK NON REINFORCED P.C.C. WITH MINIMUM 6" CRUSHED ROCK SUBBASE***
PARKING LOT: 6" THICK NON REINFORCED P.C.C. WITH MINIMUM 6" CRUSHED ROCK SUBBASE***

***6" CRUSHED ROCK SUBBASE THAT COMPLIES WITH AASHTO M147 GRADE C OR GRADE D AND SUBGRADE THAT IS COMPACTED TO 95% STANDARD PROCTOR DENSITY. GEOTECHNICAL REPORT UNAVAILABLE AT THE TIME OF CIVIL SET PREPARATION. VERIFY SUBBASE DEPTH WITH ENGINEER PRIOR TO CONSTRUCTION.

ERU CALCULATION
115,691 SQ. FT. / 4,000 SQ. FT. = 28.9 = 29
MAXIMUM ERU = 80

OPEN SPACE
EXISTING: 79% (315,702 SQ.FT.)
PROPOSED: 71% (285,474 SQ.FT.)
REQUIRED: 20% (80,233 SQ. FT.)

IMPERVIOUS SPACE
EXISTING: 21% (85,463 SQ.FT.)
PROPOSED: 29% (115,691 SQ.FT.)

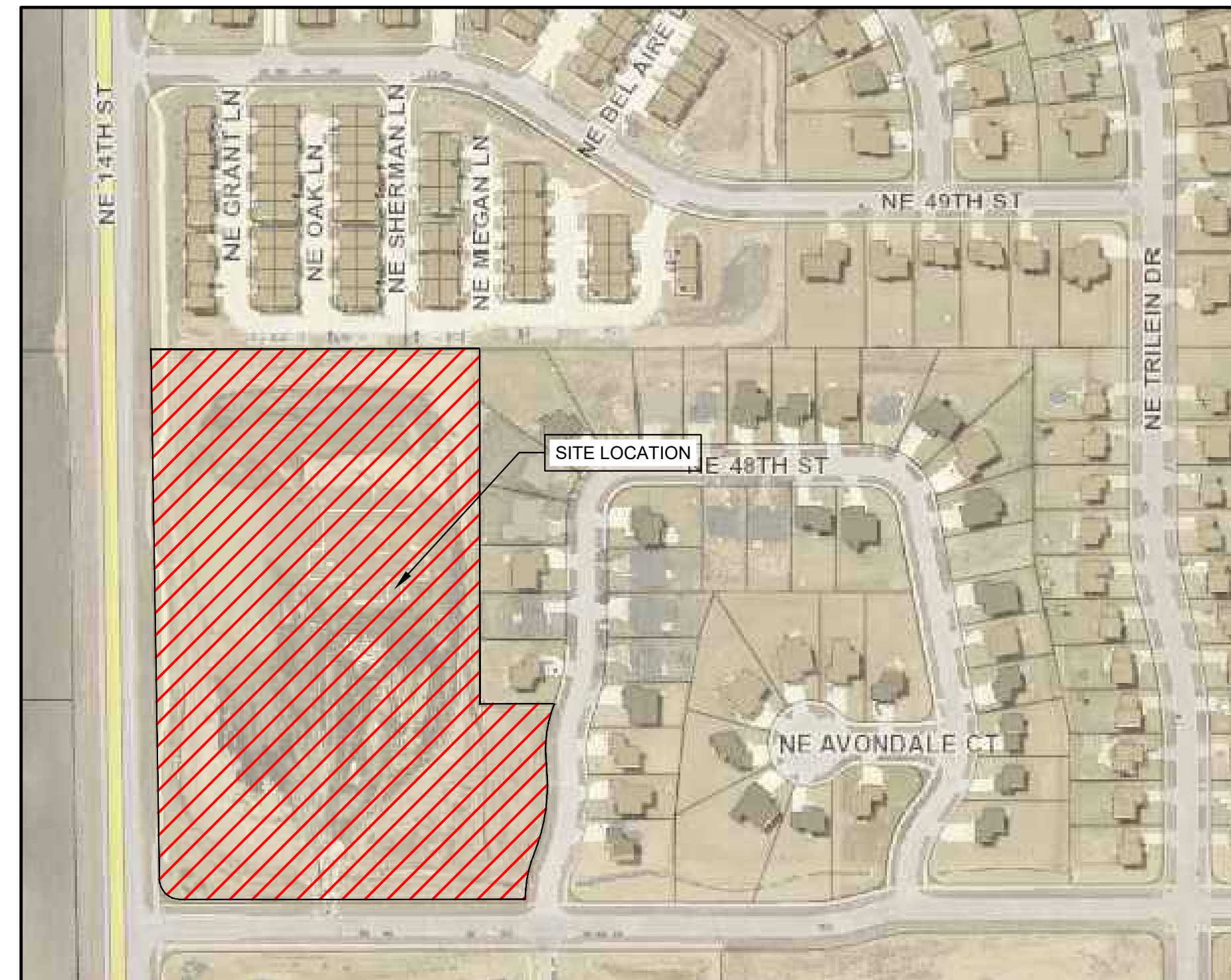
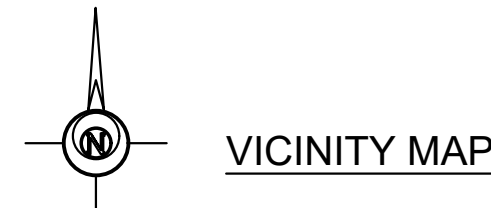
PARKING
REQUIRED: 1 SPACE FOR EVERY 80 SQ. FT. OF WORSHIP AREA PLUS ONE SPACE FOR EVERY 2 EMPLOYEES
4,857 SQ.FT. WORSHIP AREA / 80 = 61 SPACES
4 EMPLOYEES x 1 SPACES = 4 SPACES
TOTAL SPACES REQUIRED = 65 SPACES
TOTAL H.C. SPACES REQUIRED = 6 INCLUDING 1 VAN ACCESSIBLE SPACE.
TOTAL SPACES PROVIDED = 190 SPACES INCLUDING 6 H.C. SPACES

DISTURBED AREAS
ESTIMATED: 44,829 SQ.FT.
ALL DISTURBED AREAS SHALL BE SEEDED OR SODDED.
THE DISTURBED AREA FOR THIS SITE IS MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS REQUIRED.

FLOODPLAIN
PER FEMA MAP #19153C0065F, THE SITE IS LOCATED IN ZONE X, AREA OF MINIMAL FLOOD HAZARD

FAA
PER FAA NOTICE CRITERIA TOOL, SITE DOES NOT EXCEED NOTICE CRITERIA UP TO 100 FT HEIGHT (FOR POSSIBLE CRANE HEIGHT) AND THEREFORE FILING WITH THE FAA IS NOT REQUIRED

SITE IMPROVEMENTS FOR COMMUNITY BAPTIST CHURCH 4703 N ANKENY BLVD ANKENY, IA



SHEET INDEX

- C-001 - COVER SHEET
- C-002 - NOTE SHEET
- C-003 - TOPO SHEET
- C-101 - LAYOUT SHEET
- C-103 - DIMENSION SHEET
- C-201 - GRADING SHEET
- C-701 - LANDSCAPING SHEET

LEGAL DESCRIPTION:

OUTLOT "Y" EXCEPT FOR THAT PORTION CONVEYED TO IOWA DEPARTMENT OF TRANSPORTATION AS DESCRIBED IN PURCHASE AGREEMENT RECORDED IN BOOK 12855, PAGE 366 IN AVONDALE TRACE PLAT 1, AN OFFICIAL PLAT, NOW INCLUDED IN AND FORMING A PART OF THE CITY OF ANKENY, POLK COUNTY, IOWA.
AND
LOT 1 IN AVONDALE TRACE PLAT 5, AN OFFICIAL PLAT, NOW INCLUDED IN AND FORMING A PART OF THE CITY OF ANKENY, POLK COUNTY, IOWA.
AND
PARCEL "A" OF OUTLOT "Q" IN AVONDALE TRACE PLAT 5, AN OFFICIAL PLAT; ALL NOW INCLUDED IN AND FORMING A PART OF THE CITY OF ANKENY, POLK COUNTY, IOWA, THAT IS MORE PARTICULARLY DESCRIBED IN THE PLAT OF SURVEY RECORDED JANUARY 27, 2009, IN BOOK 12899, PAGE 29 WITH THE POLK COUNTY RECORDER'S OFFICE.

(BOOK 12942/PAGE 723)

DEVELOPER

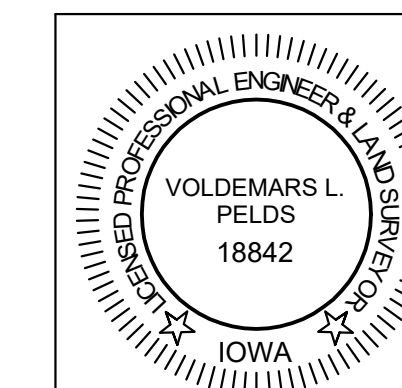
LEGACY CONSTRUCTION
3323 LINDEN DRIVE SW
BONDURANT, IA 50035
PROJECT CONTACT: DANIEL MOORE
PHONE: 319-231-1936
EMAIL: DANIEL@LEGACY-CON.COM

CITY CONTACT

CITY OF ANKENY
410 W 1ST STREET
ANKENY, IA 50023
PROJECT CONTACT: ERIC CARSTENS
PHONE: 515-963-3559
EMAIL: ECARSTENS@ANKENYIOWA.GOV

ENGINEER/ LAND SURVEYOR

PELDS DESIGN SERVICES
2323 DIXON ST
DES MOINES, IOWA 50316
PROJECT CONTACT: ELARA JONDLE
PHONE: (515) 265-8196
EMAIL: ELARA@PELDS.COM



I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

VOLDEMARS L. PELDS, P.E. IA LIC. NO. 18842 DATE _____
MY LICENSE RENEWAL DATE IS DECEMBER 31, 2025
ADDITIONAL PAGES OR SHEETS COVERED BY THIS SEAL (NONE UNLESS INDICATED HERE): C-001 - C-701

2323 Dixon Street, Des Moines, Iowa 50316 | PO Box 4626, Des Moines, Iowa 50305 | Ph: 515 265 8196

REVISIONS:

LEGEND:

F.F.	FINISHED FLOOR	▲	CALCULATED SECTION CORNER	⊕	ELEC. MANHOLE	⊞	SINGLE INTAKE	— CATV —	CABLE TELEVISION
+/-	MORE OR LESS	△	FOUND SECTION CORNER	⊖	ELEC. VAULT	⊞	AREA INTAKE	— FO —	FIBER OPTIC
123.45G	GUTTER ELEVATION	▽	PARKING SPACE	⊙	GUY ANCHOR	⊞	THROAT INTAKE	— G —	GAS LINE
123.45TC	TOP OF CURB ELEVATION	◊	SIGN	⊙	UTILITY PEDESTAL	⊞	FLARED END SECTION	— OHE —	OVERHEAD ELEC.
123.45/123.45	EXISTING/ PROPOSED ELEVATION	⊙	STREET LIGHT	⊙	CABLE TV JUNCTION BOX	⊞	VALVE	— OHT —	OVERHEAD TEL.
FL	FLOWLINE ELEVATION	⊙	POWER POLE	⊙	GAS VALVE	⊞	FIRE HYDRANT	— SAN —	SANITARY SEWER
⊙	CONTROL POINT	⊙	LIGHT POLE	⊙	GAS METER	⊞	WATER METER	— ST —	STORM SEWER
○	CALCULATED CORNER	⊙	AREA LIGHT	⊙	SANITARY SEWER MANHOLE	⊞	WATER VALVE	— UGE —	UNDERGROUND ELEC.
●	FOUND CORNER	⊙	ELEC. TRANSFORMER	⊙	STORM SEWER MANHOLE	⊞	WATER TEE	— UGT —	UNDERGROUND TEL.
		⊙	ELEC. METER	⊙	CLEANOUT	⊞	— P XX —	— W —	WATER
		⊙	ELEC. BOX	⊙	DOWNSPOUT	⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
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		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
		⊙		⊙		⊞	— E XX —	— W —	WATER
		⊙		⊙		⊞	— P XX —	— W —	WATER
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GENERAL NOTES

- THE CITY OF ANKENY MUST BE NOTIFIED BY ALL CONTRACTORS 48 HOURS PRIOR TO COMMENCING WORK.
- ALL PROPOSED MATERIALS AND STRUCTURES SHALL BE APPROVED BY THE CITY OF ANKENY AND BE INSTALLED PER MANUFACTURER'S SPECIFICATIONS
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE 2024 EDITION OF SUDAS STANDARDS AS WELL AS CITY OF ANKENY SUPPLEMENTAL SPECS.
- ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CURRENT O.S.H.A. CODES AND STANDARDS. NOTHING INDICATED ON THESE PLANS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH THE APPROPRIATE SAFETY REGULATIONS.
- ALL NECESSARY CONSTRUCTION SIGNS, BARRICADES AND OTHER TRAFFIC CONTROL DEVICES REQUIRED DURING CONSTRUCTION WILL BE FURNISHED BY THE CONTRACTOR. SIGNS, BARRICADES, AND OTHER TRAFFIC CONTROL DEVICES MUST BE IN CONFORMANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS."
- PELDS DESIGN SERVICES SHALL NOT BE LIABLE FOR ANY INJURIES THAT HAPPEN ON SITE. THIS SHALL INCLUDE BUT NOT BE LIMITED TO TRENCH COLLAPSES FROM VARYING SOIL CONDITIONS OR INJURIES CAUSED BY UNDERGROUND UTILITIES INCLUDING UTILITIES THAT ARE NOT SHOWN ON PLAN.
- THE CONTRACTOR IS LIABLE FOR ALL DAMAGES TO PUBLIC OR PRIVATE PROPERTY CAUSED BY THEIR ACTION OR INACTION IN PROVIDING FOR STORM WATER FLOW DURING CONSTRUCTION. DO NOT RESTRICT FLOWS IN EXISTING DRAINAGE CHANNELS, STORM SEWER, OR FACILITIES.
- IF A PRE-CONSTRUCTION MEETING IS REQUIRED PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE THIS MEETING.
- LABORATORY TESTS SHALL BE PERFORMED BY THE OWNER UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL PROVIDE SAMPLES OF MATERIAL REQUIRED FOR LABORATORY TESTS AND TESTING IN ACCORDANCE WITH THE URBAN STANDARD SPECIFICATIONS FOR PUBLIC IMPROVEMENTS.
- THE CONTRACTOR SHALL PROTECT ALL STRUCTURES NOT SHOWN AS REMOVALS ON THE PLANS.
- THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS PRIOR TO ANY CONSTRUCTION. CONTRACTOR SHALL WORK WITH OWNER OR OWNER'S REPRESENTATIVE ON ALL REQUIRED STORM WATER DISCHARGE PERMITS FROM THE IOWA DEPARTMENT OF NATURAL RESOURCES AND THE CITY OF ANKENY.
- THE CONTRACTOR SHALL PICK UP ANY DEBRIS SPILLED ONTO THE ADJACENT RIGHT OF WAY OR ABUTTING PROPERTIES AS THE RESULT OF CONSTRUCTION, AT THE END OF EACH WORK DAY.
- THE CONTRACTOR IS RESPONSIBLE FOR THE PROMPT REMOVAL OF ALL MUD THAT HAS BEEN TRACKED OR WASHED UNTO ADJACENT PROPERTY OR RIGHT OF WAY UNTIL SUCH TIME THAT PERMANENT VEGETATION HAS BEEN ESTABLISHED.
- DISPOSE OF ALL EXCESS MATERIALS AND TRASH IN ACCORDANCE WITH FEDERAL, STATE, AND LOCAL REQUIREMENTS. PROVIDE WASTE AREAS OR DISPOSAL SITES OR EXCESS MATERIALS NOT DESIRABLE FOR INCORPORATION INTO THE PROJECT.
- ALL CONSTRUCTION WITHIN THE PUBLIC R.O.W/EASEMENTS, AND/OR ANY CONNECTION TO PUBLIC SEWERS AND STREETS SHALL COMPLY WITH THE 2024 EDITION OF SUDAS & THE CITY OF ANKENY SUPPLEMENTAL SPECS.
- THE CONTRACTOR SHALL CONTACT THE CITY OF ANKENY TO SCHEDULE ANY REQUIRED INSPECTIONS AT LEAST ONE WEEK PRIOR TO ANY CONSTRUCTION WITHIN PUBLIC R.O.W/EASEMENTS, AND/OR ANY CONNECTION TO PUBLIC SEWERS AND STREETS
- IN THE EVENT OF ANY CONFLICTING NOTES BETWEEN THE COVER SHEET AND ALL OTHER SHEETS, THE NOTES ON THE COVER SHEET SHALL SUPERCEDE ALL OTHERS
- IN THE EVENT OF ANY CONFLICTING INFORMATION BETWEEN THE LABEL, ELECTRONIC CAD FILE, AND THE ELECTRONIC SURFACE, THE DESIGN ENGINEER SHALL BE CONTACTED FOR CLARIFICATION

SURVEY NOTES

- SURVEY WORK WAS COMPLETED BY PELDS DESIGN SERVICES IN APRIL 2024. SEE SITE SURVEY PROVIDED AS PART OF THIS SET OF PLANS FOR EXISTING SITE CONDITIONS AND BOUNDARY INFORMATION.

STAKING NOTES

- IT IS RECOMMENDED FOR THE CONTRACTOR TO HAVE ALL STAKING DONE UNDER THE SUPERVISION OF A LICENSED LAND SURVEYOR AND IN COORDINATION WITH THE PROJECT ENGINEER.
- IT IS RECOMMENDED FOR THE CONTRACTOR TO HAVE ALL STAKING DONE DIRECTLY UNDER THE GENERAL CONTRACTOR BY A SINGLE COMPANY.
- STAKING DOES NOT RELIEVE CONTRACTOR OF ULTIMATE RESPONSIBILITY TO CONSTRUCT THE PROJECT PER PLAN.

DEMO NOTES

- SAWCUT & REMOVE ALL PAVEMENT TO FULL DEPTH OF SLAB.
- PROTECT ALL TREES NOT CALLED OUT FOR REMOVAL WITH ORANGE CONSTRUCTION FENCE AT THE DRIP LINE.
- PAVEMENT STRIPPING CALLED OUT FOR REMOVAL SHALL BE SANDBLASTED. NO GRINDING WILL BE ALLOWED.
- PROPERLY DISCONNECT ALL PUBLIC AND PRIVATE UTILITIES, AS NECESSARY.
- COMPLY WITH LOCAL, STATE, AND FEDERAL REGULATIONS TO REMOVE AND PROPERLY DISPOSE OF SIDEWALKS, STEPS, DRIVEWAYS, AND STRUCTURES (INCLUDING BOTH ABOVE GROUND AND BELOW GROUND ELEMENTS).
- COMPLY WITH LOCAL, STATE, AND FEDERAL REGULATIONS TO REMOVE FUEL TANKS, SEPTIC TANKS, CISTERNS, AND ANY OTHER UNDERGROUND FACILITIES; AND TO PROPERLY DISPOSE OF ANY LIQUIDS OR PRODUCTS CONTAINED WITHIN THESE ITEMS.
- PLACE BACKFILL MATERIAL IN HOLES AND DEPRESSIONS, GRADE THE SITE, AND ESTABLISH GROUND COVER.
- PROTECT EXISTING FIRE HYDRANTS, STREET LIGHTS, TRAFFIC SIGNALS, UTILITY POLES, FIRE ALARM BOXES, WIRE CABLES, UNDERGROUND UTILITIES, AND OTHER APPURTENANCES IN THE VICINITY OF THE DEMOLITION SITE
- COMPLY WITH NOISE POLLUTION REQUIREMENTS AND ANY WORKING HOUR RESTRICTIONS OF THE JURISDICTION.
- MAINTAIN OR RE-ESTABLISH ALL TILES, ROADWAY SUBDRAINS, CULVERTS, OR OTHER DRAINAGE FACILITIES NOT IDENTIFIED IN THE CONTRACT DOCUMENTS FOR REMOVAL.
- IMPLEMENT THE APPROVED EROSION AND SEDIMENT CONTROL PLAN FOR EACH SITE PRIOR TO INITIATING DEMOLITION BY PLACING ALL REQUIRED DEVICES; INCLUDE MEASURES TO PREVENT TRACKING OF MUD ONTO ADJACENT STREETS OR ALLEYS.
- COMPLY WITH ALL APPLICABLE AIR POLLUTION REQUIREMENTS OF THE JURISDICTION. USE WATER OR APPROPRIATE CHEMICALS FOR CONTROL OF DUST IN THE DEMOLITION AREA, ON HAULING EQUIPMENT, ON ADJACENT ROADWAYS, AND WHEN GRADING THE SITE.
- TAKE STEPS TO PREVENT THE GENERATION OF LITTER DURING DEMOLITION AND COLLECT ALL LITTER FROM THE DEMOLITION AREA AT THE END OF EACH WORKING DAY. LOAD TRUCKS TO PREVENT LEAKAGE OR BLOWING OF DEBRIS.
- REMOVE ALL CONCRETE, ASPHALT, OR MASONRY SLABS AND APPURTENANCES.
- REMOVE AND DISPOSE OF ALL BRUSH, SHRUBS, TREES, LOGS, DOWNED TIMBER, AND OTHER YARD WASTE ON THE SITE UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS. DO NOT MIX WITH DEMOLITION MATERIAL. REMOVE STUMPS TO A MINIMUM OF 2 FEET BELOW FINISH GRADE. PROTECT ANY TREES OR OTHER VEGETATION NOT DESIGNATED FOR REMOVAL BY PLACING A FENCE AT THE DRIP LINE ENCOMPASSING THE ENTIRE TREE AND KEEPING ALL OPERATIONS OUTSIDE OF THE FENCED IN AREA, INCLUDING STORAGE OF EQUIPMENT OR MATERIALS. AT NO ADDITIONAL COST TO THE CONTRACTING AUTHORITY, REPLACE ANY TREES THAT ARE DESIGNATED FOR PROTECTION BUT ARE DAMAGED BEYOND TREATMENT. THE ENGINEER WILL DETERMINE SIZE AND SPECIES OF THE REPLACEMENT TREE.
- SAW-CUT ALL HARD SURFACING AT LOCATION OF REMOVAL LIMITS.
- WHERE SELECTIVE DEMOLITION IS SHOWN AT PAVED AREAS, DEMOLISH THE EXISTING PAVING BACK TO AN EXISTING CONTROL JOINT NEAR THE LOCATION SHOWN.

WETLAND NOTES

- PELDS DESIGN SERVICES DOES NOT PERFORM WETLAND STUDIES OR WETLAND MITIGATION. IT IS THE OWNER'S RESPONSIBILITY TO DETERMINE IF ANY WETLANDS ARE LOCATED ON THE PROJECT SITE AND PERFORM ANY NECESSARY MITIGATION PRIOR TO THE COMMENCEMENT OF CONSTRUCTION ACTIVITIES.

DISCLAIMER

- THIS DRAWING IS BEING MADE AVAILABLE BY PELDS DESIGN SERVICES (P.D.S.) FOR USE ON THIS PROJECT IN ACCORDANCE WITH P.D.S.'S AGREEMENT FOR PROFESSIONAL SERVICES. P.D.S. ASSUMES NO RESPONSIBILITY OR LIABILITY (CONSEQUENTIAL OR OTHERWISE) FOR ANY USE OF THESE DRAWINGS (OR ANY PART THEREOF) EXCEPT IN ACCORDANCE WITH THE TERMS OF SAID AGREEMENT.
- P.D.S. DISCLAIMS ANY AND ALL LIABILITY OR RESPONSIBILITY FOR INFORMATION THAT DOES NOT BEAR THE ORIGINAL SEAL AND SIGNATURE OF THE PROFESSIONAL IN CHARGE.

CIVIL ENGINEERING NOTES FOR COMMUNITY BAPTIST CHURCH 4703 N ANKENY BLVD ANKENY, IA

UTILITY NOTES

- QUANTITY CALLOUTS ON PIPE LENGTHS ARE APPROXIMATE AND SHOULD BE USED FOR REFERENCE ONLY.
- PIPE LENGTHS CALLED OUT ON PLANS DO NOT INCLUDE FLARED END SECTION, WHERE APPLICABLE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION OF ALL UTILITIES. ANY DAMAGE TO SAID UTILITIES SHALL BE REPAIRED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL RELOCATE EXISTING UTILITIES AS NECESSARY, SHOWN OR NOT SHOWN.
- THE CONTRACTOR SHALL COORDINATE WITH THE CITY ON ALL SIZE, DEPTHS, AND MATERIALS OF ALL PROPOSED UTILITIES.
- THE CONTRACTOR MUST PROVIDE AS-BUILTS OF ALL UTILITIES, INCLUDING DEPTH AND LOCATION OF ALL SERVICES IF REQUIRED BY CITY.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE APPROPRIATE UTILITY COMPANIES PRIOR TO CONNECTION OR DISCONNECTION OF A SERVICE LINE.
- CONTRACTOR IS TO COMPLY WITH THE 2024 EDITION OF SUDAS & CITY OF ANKENY SUPPLEMENTAL SPECIFICATIONS FOR MAINTENANCE, INSTALLATION, AND TESTING FOR CONSTRUCTION.
- THE CONTRACTOR SHALL COORDINATE THE ADJUSTMENT OF ANY AND ALL EXISTING AND PROPOSED UTILITIES TO PROPOSED GRADES. EXISTING UTILITIES SHALL BE RAISED OR LOWERED IN ACCORDANCE WITH THE UTILITY OWNER REQUIREMENTS. ANY NECESSARY ADJUSTMENTS SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION.
- ACTIVE EXISTING FIELD TILES ENCOUNTERED DURING CONSTRUCTION SHALL BE REPAIRED, REROUTED, OR CONNECTED TO PUBLIC OR PRIVATE STORM SEWER TO REMAIN IN SERVICE. THE TILE SHOULD BE DIRECTED TO PUBLIC STORM SEWER IF POSSIBLE. THE CONTRACTOR SHALL RECORD THE ELEVATION AND LOCATION OF ALL TILES.
- ALL RIP RAP CALLED OUT ON PLANS SHALL BE UNDERLAIN WITH ENGINEERING FABRIC.
- ALL STRUCTURE SIZES CALLED OUT ON PLANS ARE MINIMUM INSIDE WALL DIMENSIONS.
- ALL PROPOSED UTILITIES SHALL BE PRIVATE, UNLESS NOTED OTHERWISE.
- ALL STRUCTURES CALLED OUT AS "MODIFIED" OR "SPECIAL" SHALL BE ENGINEERED BY THE PRECASTER TO ENSURE STRUCTURE STABILITY. MODIFICATIONS INCLUDE ADDITIONAL WALL LENGTH TO ACCOUNT FOR LARGE PIPE DIAMETERS AND ADDITIONAL WALL WIDTH & BASE DEPTH TO ACCOUNT FOR DEEPER STRUCTURES. ANY AND ALL MODIFICATIONS TO STANDARD STRUCTURES SHALL BE CONSIDERED INCIDENTAL TO BID

ELECTRICAL SERVICE NOTES

- ALL SITE LIGHTING SHALL NOT SPILL ONTO ADJACENT PROPERTIES OR RIGHT-OF-WAYS
- TRANSFORMER PAD IS SHOWN FOR REFERENCE ONLY AND MAY NOT BE DRAWN TO ACTUAL SIZE. PAD SHALL CONFORM TO ALL UTILITY COMPANY DESIGN STANDARDS AND SPECIFICATIONS.

SANITARY SERVICE NOTES

- SANITARY SEWER SERVICE CONNECTIONS SHALL BE PLACED AT A SLOPE OF NO LESS THAN 2% FOR A 4" PIPE AND 1% FOR A 6" PIPE. SERVICES SHALL MAINTAIN 18" OF VERTICAL SEPARATION FROM THE WATER MAIN WITH 18" OF COMPACTED LOW PERMEABILITY SOIL BETWEEN THE UTILITIES WITHIN 10' OF THE CROSSING.

STORM WATER SERVICE NOTES

- ALL STORM SEWER IN THE ROW SHALL BE RCP (CLASS III RCP), UNLESS OTHERWISE SPECIFIED.
- ALL PROPOSED RCP STORM SEWER PIPE JOINTS SHALL BE FABRIC WRAPPED AND THE LAST 3 PIPE SECTIONS ON THE APRON SHALL BE TIED WITH RF-14 TYPE II CONNECTORS. ALL APRONS SHALL HAVE A STANDARD FOOTING AND TRASH GUARD.
- ALL SUMP SERVICE LINES SHALL HAVE TRACE WIRE

WATER SERVICE NOTES

- WATER SERVICE SHALL BE TYPE K COPPER, DIP OR HDPE, DEPENDING ON SIZE AND JURISDICTIONAL REQUIREMENTS. IF CONTAMINATED OR "HOT" SOILS, ENSURE APPROPRIATE PIPE IS BEING UTILIZED. SIZE OF WATER MAIN AS SHOWN ON PLANS.
- TRACER WIRE SHALL BE ADDED TO ALL WATER MAIN, AND BROUGHT TO THE SURFACE AT EVERY HYDRANT.
- THRUST BLOCKS SHALL BE INSTALLED AS REQUIRED AND SHALL BE CONSIDERED INCIDENTAL TO WATER MAIN CONSTRUCTION.
- ANY AND ALL HYDRANT AND VALVE EXTENSIONS, TOGETHER WITH VERTICAL BENDS, SHALL BE CONSIDERED INCIDENTAL TO WATER MAIN CONSTRUCTION. NO ADDITIONAL PAYMENT WILL BE PROVIDED FOR INCIDENTAL ITEMS.
- ALL WATER LINES SHALL HAVE A MINIMUM OF 5.5 FEET OF COVER UNLESS NOTED OTHERWISE.

EXISTING UTILITIES NOTE

- THE LOCATIONS OF THE EXISTING UTILITIES SHOWN HEREON ARE APPROXIMATE. THEY HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND/OR RECORDS. THE CONTRACTOR IS RESPONSIBLE TO ENSURE THAT ANY EXISTING UTILITIES (SHOWN OR NOT SHOWN) ARE NOT DAMAGED DURING CONSTRUCTION: IOWA ONE-CALL (1-800-292-8989).

UTILITY CONFLICT NOTES

- UTILITY CONFLICTS MAY EXIST ACROSS THE SITE WITH NEW UTILITIES, GRADING, PAVING ETC. MOST UTILITY CONFLICTS HAVE BEEN CALLED OUT FOR CONTRACTOR CONVENIENCE.
- CONTRACTOR IS RESPONSIBLE FOR ALL UTILITY CONFLICTS BETWEEN AN EXISTING UTILITY AND PROPOSED CONSTRUCTION THAT ARE EITHER CALLED OUT ON THE PLANS OR NOT CALLED OUT.
- CONTRACTOR SHALL COMPLY WITH ALL DNR REQUIREMENTS FOR PIPE MATERIAL, PIPE JOINTS, AND ANY OTHER APPLICABLE REQUIREMENTS ANY TIME A STORM SEWER OR SANITARY SEWER CROSSES OVER OR LESS THAN 18" BELOW A WATER MAIN.
- FOR ALL CRITICAL CROSSINGS WITH EXISTING UTILITIES, THE ELEVATION OF THE EXISTING UTILITY SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION. NOTIFY PELDS DESIGN SERVICES PRIOR TO CONSTRUCTION IF A CONFLICT EXISTS.

SPECIFICATIONS NOTES

- ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE 2024 EDITION OF SUDAS FOR PUBLIC IMPROVEMENTS ON THE DATE OF APPROVAL AND THE CITY OF ANKENY SUPPLEMENTAL SPECIFICATIONS.
- IN THE EVENT OF A DISCREPANCY BETWEEN THE PROJECT SPECIFICATIONS AND THE CITY OF ANKENY REQUIREMENTS AND SPECIFICATIONS, PLUMBING CODE, AND URBAN STANDARD SPECIFICATIONS FOR PUBLIC IMPROVEMENT, THE CITY OF ANKENY SUPPLEMENTAL SPECIFICATIONS SHALL GOVERN.
- FOR ALL SPECIFICATION DISCREPANCIES, THE PROJECT ENGINEER SHALL BE CONTACTED PRIOR TO PROCEEDING WITH CONSTRUCTION. IF THE ENGINEER IS NOT CONTACTED, CONTRACTOR SHALL BE RESPONSIBLE FOR ANY PROBLEMS THAT RESULT FROM SAID DISCREPANCIES.

REQUIRED AS-BUILT NOTES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COST OF AS-BUILT TOPO, IF REQUIRED BY CITY.
- CONTRACTOR SHALL CONTACT PELDS DESIGN SERVICES TO PERFORM SAID AS-BUILT SURVEYS.
- IF ANYTHING HAS BEEN CONSTRUCTED INCORRECTLY, CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SUBSEQUENT AS-BUILT SURVEYS UNTIL ISSUES HAVE BEEN RECTIFIED.

PAVING NOTES

- THE PAVING/ GRADING CONTRACTOR SHALL BACKFILL THE PAVING SLAB AND FINE GRADE THE RIGHT OF WAY AS SOON AFTER THE PAVING AS POSSIBLE. ALL ROW AREAS SHALL BE SODDED IN ACCORDANCE WITH CITY OF ANKENY SPECIFICATIONS AND THE LATEST VERSION OF SUDAS.
- SEE DETAILS FOR ALL PAVEMENT THICKNESS.
- DETECTABLE WARNING PANEL(S) CALLED OUT ON PLANS SHALL BE PER A.D.A. REGULATIONS. PANEL TYPE & COLOR SHALL BE PER CITY STANDARD.
- SIDEWALKS SHALL HAVE EXPANSION JOINTS AT 100 FOOT INTERVALS AS REQUIRED BY CITY CODE
- ALL WALKS, PARKING LOTS, HANDICAP PARKING, RAMPS, ETC. SHALL COMPLY WITH ALL A.D.A. AND CITY CODES. HANDICAP PARKING SIGNAGE IS REQUIRED FOR ALL HANDICAP STALLS AND SHALL BE CONSIDERED INCIDENTAL. IN EVENT OF A DISCREPANCY BETWEEN THE PLANS AND THE A.D.A./CITY CODES THE A.D.A./CITY CODES SHALL GOVERN. CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING A.D.A. CODES ARE MET.

PAVEMENT SAWCUT NOTES

- CONTRACTOR TO PROVIDE SAWCUT JOINTING PLAN PRIOR TO ANY CONCRETE PAVEMENT INSTALLATION.
- PAVEMENT MAY BE REQUIRED TO BE REMOVED AND REPLACED IF PLACED WITHOUT AN APPROVED PLAN.
- LONGITUDINAL JOINTS IN DRIVE LANES & THE OUTER MOST JOINT OF ALL PARKING AREAS SHALL BE SUDAS TYPE 'L-1' OR 'L-2' JOINTS AND HAVE STEEL INTERIOR PARKING LOT JOINTS, OTHER THAN THE OUTER MOST JOINT, DO NOT NEED STEEL AND SHALL BE SUDAS TYPE 'B' OR TYPE 'K' JOINTS. ALL TRANSVERSE JOINTS SHALL BE SUDAS TYPE 'C' OR TYPE 'DW' JOINTS WITH STEEL IN THE CASE OF A DAY'S WORK JOINT.

SOIL NOTES

- STRIP AND STOCKPILE THE TOP 8 INCHES OF SOIL ON ALL DISTURBED AREAS. GRADING CONTRACTOR SHALL STOCKPILE TOPSOIL FOR SHOULDERING.
- SOD REQUIREMENT PER SUDAS: IN AREAS WHERE TOPSOIL HAS BEEN STRIPPED, A SODBED SHOULD BE CONSTRUCTED BY SPREADING A MINIMUM OF 6 INCHES OF TOPSOIL PRIOR TO SODDING. DEEPER TOPSOIL DEPTHS (8 TO 12 INCHES OR GREATER) ARE DESIRABLE AS THEY INCREASE THE ORGANIC MATTER AVAILABLE FOR USE BY THE PLANTS, ALLOW FOR DEEPER ROOT PENETRATION AND INCREASE THE MOISTURE HOLDING ABILITY OF THE SOIL.
- PERMANENT SEED REQUIREMENT PER SUDAS: IN ORDER TO PROVIDE AN ADEQUATE GROWING MEDIUM, A MINIMUM OF 8 INCHES OF TOPSOIL SHOULD BE PLACED OVER THE DISTURBED AREA PRIOR TO SEEDING. DEEPER TOPSOIL DEPTHS (8-12 INCHES OR GREATER) ARE DESIRABLE AS THEY INCREASE THE ORGANIC MATTER AVAILABLE FOR USE BY THE PLANTS, ALLOW FOR DEEPER ROOT PENETRATION AND INCREASE THE MOISTURE HOLDING ABILITY OF THE SOIL.
- TOPSOIL SHALL BE FREE OF ALL ROCK AND DEBRIS LARGER THAN 3/4" IN SIZE.
- TOPSOIL IS DEFINED AS: FERTILE, FRIABLE LOAM, CAPABLE OF SUSTAINING VIGOROUS PLANT GROWTH, FROM WELL DRAINED SITE FREE FROM FLOODING, NOT IN FROZEN OR MUDDY CONDITIONS; REASONABLE FREE FROM SUBSOIL, CLAY LUMPS, ROOTS, GRASS, WEEDS, STONES LARGER THAN 3/4 INCH IN DIAMETER, AND FOREIGN MATTER; ACIDITY RANGE (PH) OF 5.5 TO 7.5; CONTAINING MINIMUM 4 PERCENT AND MAXIMUM 20 PERCENT ORGANIC MATTER.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING ALL TOPSOIL REQUIREMENTS OF NPDES GENERAL PERMIT NO. 2 ARE MET.
- REFERENCE GEOTECHNICAL REPORT FOR ADDITIONAL SOILS INFORMATION INCLUDING BUT NOT LIMITED TO SUBGRADE INFORMATION, FOOTING DESIGN, AND ANY POSSIBLE OVER EXCAVATION DUE TO POOR SOILS. PAVEMENT SECTIONS ON THE PLANS GOVERN OVER PAVEMENT THICKNESS IN THE SOILS REPORT.

GRADING NOTES

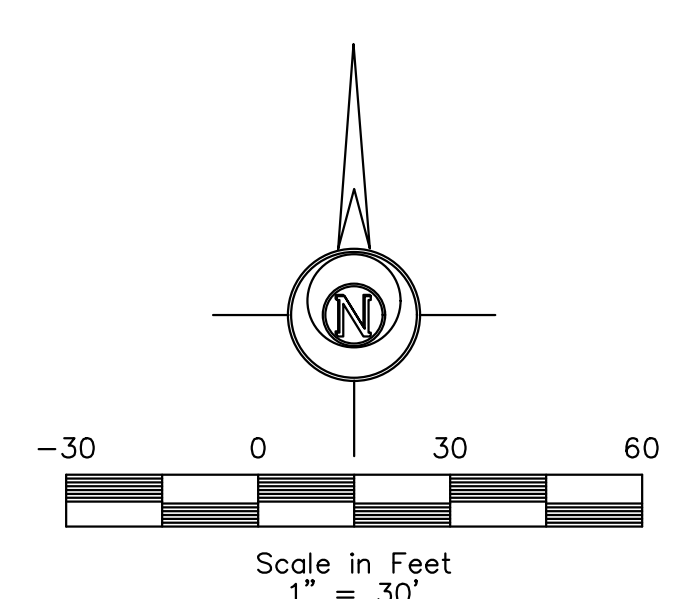
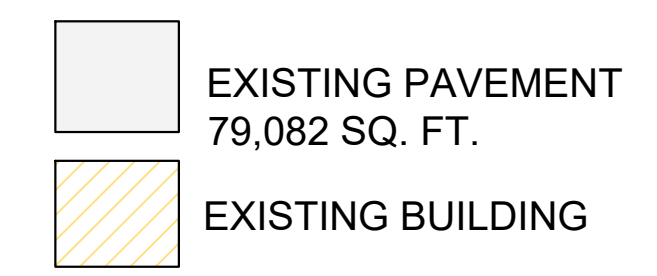
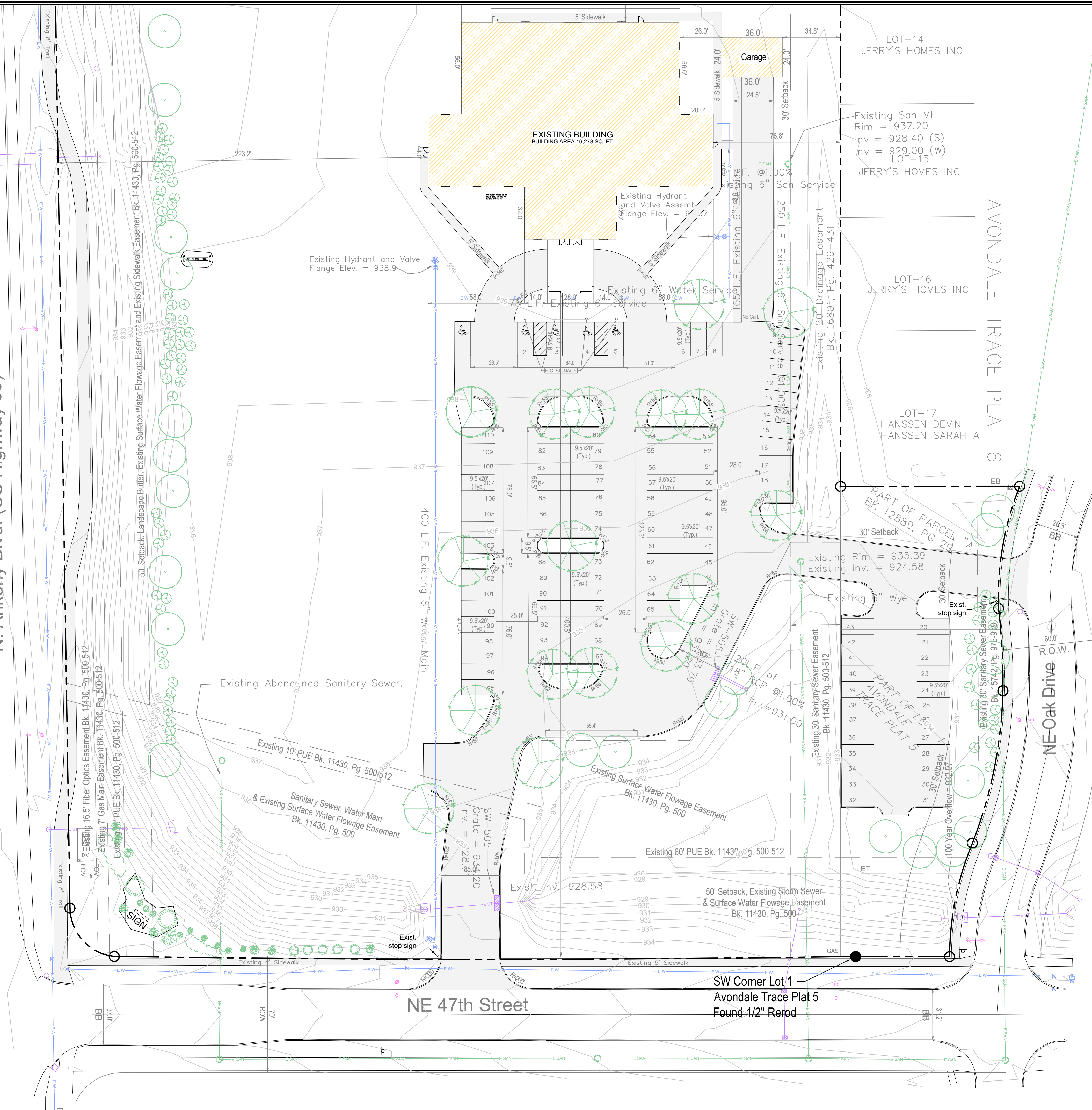
- AREAS TO RECEIVE STRUCTURAL FILL SHALL BE BENCHED. STRUCTURAL FILL SHALL BE COMPACTED TO A DENSITY THAT IS NOT LESS THAN 95% STANDARD PROCTOR.
- PREPARE BOTTOM OF BENCH FOR FILL BY DISKING TO A DEPTH OF 12 INCHES AND COMPACT. ANY LOCALIZED AREAS WHICH CANNOT BE SATISFACTORILY COMPACTED OR WHICH SHOW EVIDENCE OF PUMPING ACTION SHALL BE UNDERCUT AND RE-COMPACTED WITH ON-SITE FILL MATERIALS.
- ALL AREAS WHICH ARE TO RECEIVE PAVING SHALL HAVE THE TOP 12 INCHES DISKED AND COMPACTED TO 95% STANDARD PROCTOR DENSITY.
- MAINTAIN ALL CUT AND FILL AREAS TO ACCOMMODATE SURFACE DRAINAGE.
- FINISH GRADE ON ALL NON-PAVED AREAS SHALL BE WITHIN 0.20 FT, AND PAVED AREAS SHALL BE WITHIN 0.10 FT OF THE PROPOSED GRADES SHOWN ON THE PLAN
- MAXIMUM ALLOWABLE GRADE ON ALL PROPOSED SLOPES IS 3:1.
- THE MOISTURE CONTENT SHALL NOT DEVIATE FROM THE OPTIMUM BY MORE THAN 2% IN STRUCTURAL FILL.
- MAXIMUM 2% CROSS-SLOPE ON ALL SIDEWALKS.
- GRADING AND EROSION CONTROL SHALL BE DONE IN ACCORDANCE WITH THE APPROVED GRADING PLAN, SWPPP, NPDES DOCUMENTS, AND IOWA DEPARTMENT OF NATURAL RESOURCES REQUIREMENTS.

LANDSCAPING NOTES

- ALL SODDING & LANDSCAPE PLANTINGS SHALL BE INSTALLED IN ACCORDANCE WITH THE URBAN STANDARD SPECIFICATIONS, UNLESS SPECIFIED OTHERWISE.
- SEED OR SOD ALL DISTURBED AREAS WITHIN THE CONTRACT LIMITS, UNLESS NOTED OTHERWISE. SOD LIMITS SHOWN ON PLAN ARE FOR REFERENCE ONLY. FINAL LIMITS MAY CHANGE BASED ON CONSTRUCTION ACTIVITIES. SEED ALL DISTURBED AREAS OFF-SITE
- STAKE SOD ON ALL SLOPES 4:1 OR GREATER.
- PLANT QUANTITIES ARE FOR CONTRACTORS CONVENIENCE, THE DRAWING SHALL PREVAIL IF A CONFLICT OCCURS.
- ALL PLANT MATERIAL SHALL CONFORM TO THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK (ANSI Z60.1).
- CONTRACTOR SHALL GUARANTEE ALL PLANT MATERIAL FOR A PERIOD OF ONE YEAR FROM THE DATE OF ACCEPTANCE.
- CONTRACTOR SHALL PLACE SHREDDED HARDWOOD MULCH OR BRICK CHIPS OR WASHED STONE AROUND ALL TREES, SHRUBS AND GROUND COVER BEDS TO A DEPTH OF 4 INCHES, UNLESS NOTED.
- PROVIDE 6MIL MINIMUM PLASTIC BELOW ROCK/BRICK.
- THE LANDSCAPING CONTRACTOR SHALL HAVE ALL UTILITIES LOCATED BEFORE STARTING ANY SITE WORK OR PLANTING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES.
- CONTRACTOR SHALL BE RESPONSIBLE MAINTAINING APPROPRIATE LEVEL OF WATERING FOR ALL NEW PLANTS FOR A PERIOD OF 30 DAYS.
- CONTRACTOR SHALL DESIGN AND PROVIDE IRRIGATION SYSTEM. COORDINATION WITH ARCHITECT REQUIRED.
- ALL BEDS TO RECEIVE GRANULAR PRE-EMERGENT WEED CONTROL BEFORE AND AFTER MULCH IS INSTALLED.

 PELDS Design Services Architecture Engineering Surveying 2323 Dixon Street, Des Moines, Iowa 50316 PO Box 4626, Des Moines, Iowa 50305 Ph: 515 265 8196			
TITLE: COMMUNITY BAPTIST CHURCH 4703 N ANKENY BLVD ANKENY, IA			
REVISIONS:		REVISIONS:	
COPYRIGHT DATE: ----	DRAWN BY: L. BURKE	SCALE:	DATE REVISION: 06.25.2024
PROJECT NO: 24-039			ISSUE NO: 24-039
PROJECT: 24-039 Community Baptist Church/Engineering/Sheet Set			C-002 - NOTE SHEET

N. Ankeny Blvd. (US Highway 69)



LEGEND:

+/-	MORE OR LESS	⊙	ELEC. MANHOLE
F.F.	FINISHED FLOOR	⊙	SANITARY SEWER MANHOLE
123.45G	GUTTER ELEVATION	⊙	STORM SEWER MANHOLE
123.45TC	TOP OF CURB ELEVATION	⊙	TRAFFIC MANHOLE
123.45/123.45	EXISTING/PROPOSED ELEVATION	⊙	CLEANOUT
FL	FLOWLINE ELEVATION	⊙	DOWNSPOUT
○	CONTROL POINT	⊙	AREA INTAKE
○	CALCULATED CORNER	⊙	SINGLE INTAKE
●	FOUND CORNER	⊙	THROAT INTAKE
○	CALCULATED SECTION CORNER	⊙	FLARED END SECTION
○	FOUND SECTION CORNER	⊙	GAS VALVE
□	PARKING SPACE	⊙	FIRE HYDRANT
□	SIGN	⊙	WATER VALVE
□	STREET LIGHT	⊙	PROPOSED UTILITY LINE
□	POWER POLE	⊙	EXISTING UTILITY LINE
□	LIGHT POLE	⊙	CATV - CABLE TELEVISION
□	AREA LIGHT	⊙	FO - FIBER OPTIC
□	GUY ANCHOR	⊙	GAS - GAS LINE
□	UTILITY PEDESTAL	⊙	OHE - OVERHEAD ELEC.
□	ELEC. TRANSFORMER	⊙	OHT - OVERHEAD TEL.
□	ELEC. METER	⊙	SS - SANITARY SEWER
□	ELEC. BOX	⊙	ST - STORM SEWER
□	ELEC. VAULT	⊙	UGE - UNDERGROUND ELEC.
□	FIBER OPTIC VAULT	⊙	UGT - UNDERGROUND TEL.
□	GAS METER	⊙	W - WATER
□	CABLE TV JUNCTION BOX	⊙	— — — — — FENCE LINE
□	WATER METER		

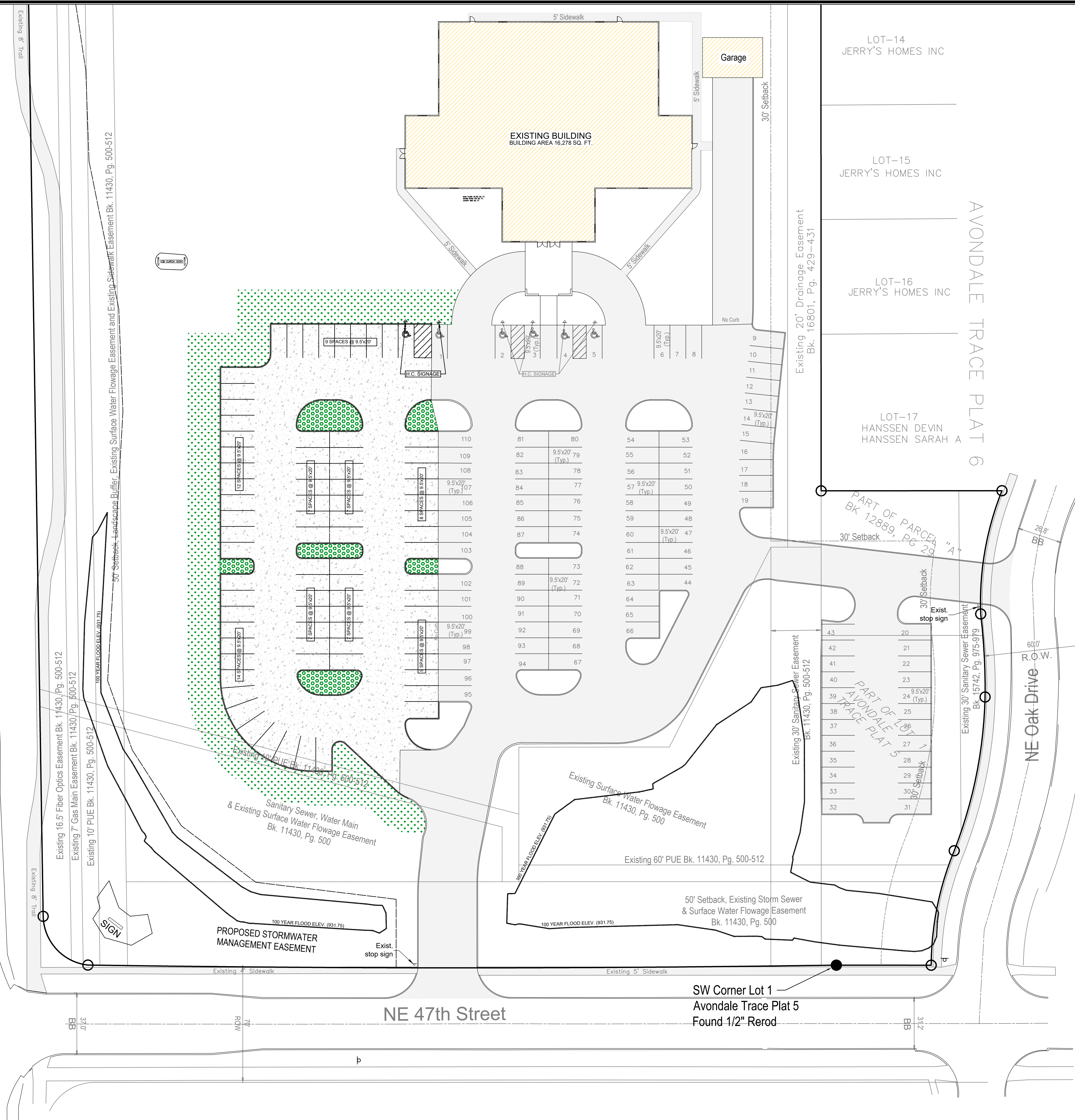
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COMMUNITY BAPTIST CHURCH
4703 N ANKENY BLVD
ANKENY, IA

PRELIMINARY

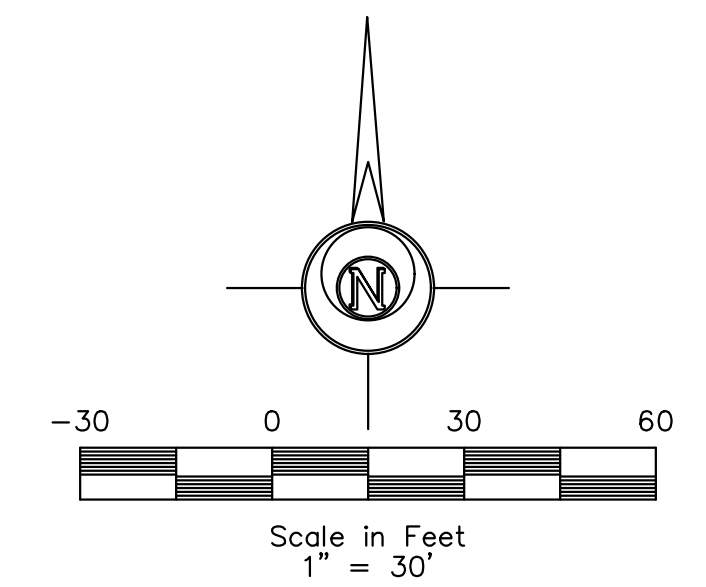
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DRAWN BY: L. BURKE
SCALE: 1"=30'
LATEST REVISION: 24-039
DRAWING NO: C-003 - TOPO SHEET

N. Ankeny Blvd. (US Highway 69)



NOTES:
 ALL WORK IN/ON THE R.O.W. AREA IS SUBJECT TO THE CITY OF ANKENY APPROVAL AND SPECIFICATIONS
 CONTRACTOR IS RESPONSIBLE FOR DESIGN AND INSTALL OF IRRIGATION SYSTEM

- PROPOSED 6" MIN. STANDARD PAVEMENT
30,054 SQ. FT.
- PROPOSED SOD/SEED
11,202 SQ. FT.
- PROPOSED LANDSCAPING ROCK
3,271 SQ. FT.
- EXISTING PAVEMENT
79,082 SQ. FT.
- EXISTING BUILDING



LEGEND:

- | | | | |
|---------------|------------------------------|---|----------------------------|
| +/- | MORE OR LESS | ⊙ | ELEC. MANHOLE |
| F.F. | FINISHED FLOOR | ⊙ | SANITARY SEWER MANHOLE |
| 123.45G | GUTTER ELEVATION | ⊙ | STORM SEWER MANHOLE |
| 123.45TC | TOP OF CURB ELEVATION | ⊙ | TRAFFIC MANHOLE |
| 123.45/123.45 | EXISTING/ PROPOSED ELEVATION | ⊙ | CLEANOUT |
| FL | FLOWLINE ELEVATION | ⊙ | DOWNSPOUT |
| ⊙ | CONTROL POINT | ⊙ | AREA INTAKE |
| ⊙ | CALCULATED CORNER | ⊙ | SINGLE INTAKE |
| ⊙ | FOUND CORNER | ⊙ | THROAT INTAKE |
| ⊙ | CALCULATED SECTION CORNER | ⊙ | FLARED END SECTION |
| ⊙ | FOUND SECTION CORNER | ⊙ | GAS VALVE |
| ⊙ | PARKING SPACE | ⊙ | FIRE HYDRANT |
| ⊙ | SIGN | ⊙ | WATER VALVE |
| ⊙ | STREET LIGHT | ⊙ | P XX PROPOSED UTILITY LINE |
| ⊙ | POWER POLE | ⊙ | E XX EXISTING UTILITY LINE |
| ⊙ | LIGHT POLE | ⊙ | CATV CABLE TELEVISION |
| ⊙ | AREA LIGHT | ⊙ | FO FIBER OPTIC |
| ⊙ | GUY ANCHOR | ⊙ | GAS GAS LINE |
| ⊙ | UTILITY PEDESTAL | ⊙ | OHE OVERHEAD ELEC. |
| ⊙ | ELEC. TRANSFORMER | ⊙ | OHT OVERHEAD TEL. |
| ⊙ | ELEC. METER | ⊙ | SAN SANITARY SEWER |
| ⊙ | ELEC. BOX | ⊙ | ST STORM SEWER |
| ⊙ | ELEC. VAULT | ⊙ | UGE UNDERGROUND ELEC. |
| ⊙ | FIBER OPTIC VAULT | ⊙ | UGT UNDERGROUND TEL. |
| ⊙ | GAS METER | ⊙ | W WATER |
| ⊙ | CABLE TV JUNCTION BOX | ⊙ | ⊙ FENCE LINE |
| ⊙ | WATER METER | | |

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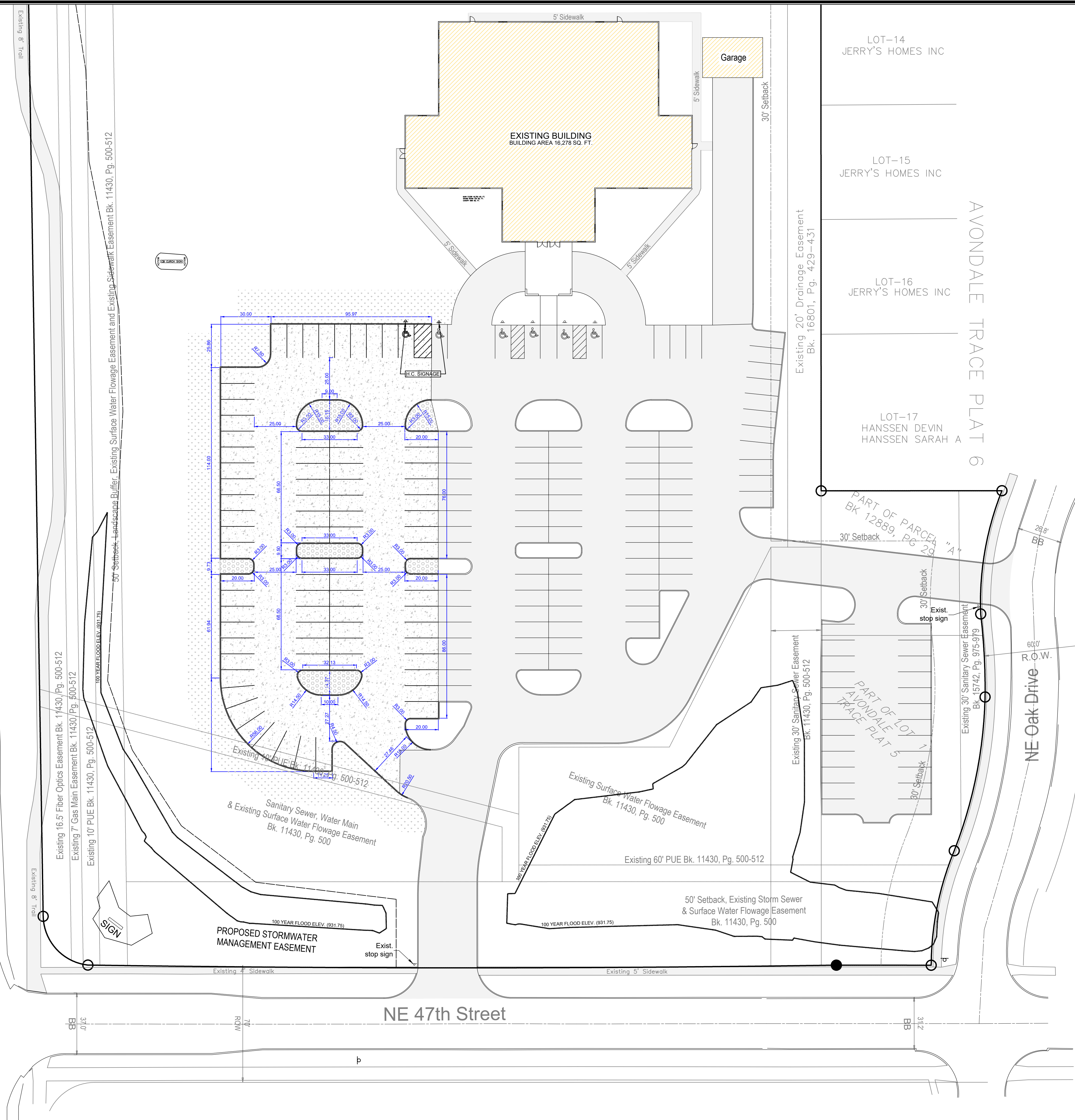
COMMUNITY BAPTIST CHURCH
 4703 N ANKENY BLVD
 ANKENY, IA

PRELIMINARY

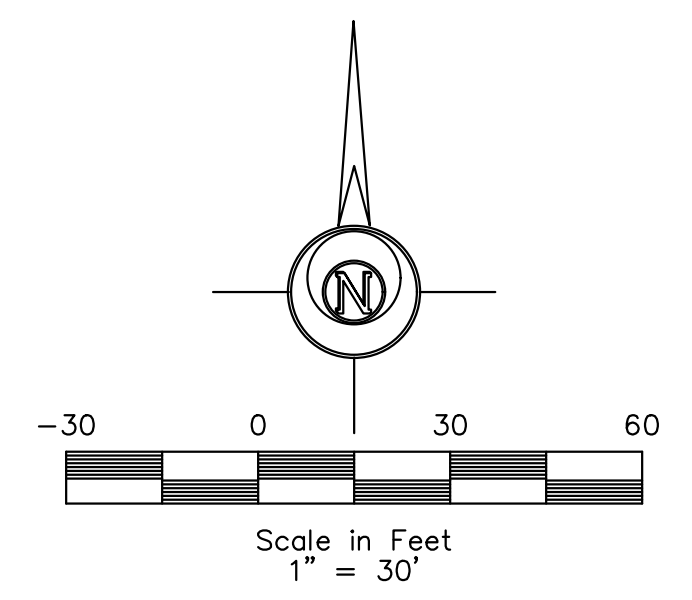
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C-101 - LAYOUT SHEET

N. Ankeny Blvd. (US Highway 69)



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30,054 SQ. FT.
- PROPOSED SOD/SEED
11,202 SQ. FT.
- PROPOSED LANDSCAPING ROCK
3,271 SQ. FT.
- EXISTING PAVEMENT
79,082 SQ. FT.
- EXISTING BUILDING



LEGEND:

+/-	MORE OR LESS		ELEC. MANHOLE
F.F.	FINISHED FLOOR		SANITARY SEWER MANHOLE
123.45G	GUTTER ELEVATION		STORM SEWER MANHOLE
123.45TC	TOP OF CURB ELEVATION		TRAFFIC MANHOLE
123.45/123.45	EXISTING/ PROPOSED ELEVATION		CLEANOUT
FL	FLOWLINE ELEVATION		DOWNSPOUT
	CONTROL POINT		AREA INTAKE
	CALCULATED CORNER		SINGLE INTAKE
	FOUND CORNER		THROAT INTAKE
	CALCULATED SECTION CORNER		FLARED END SECTION
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	PARKING SPACE		FIRE HYDRANT
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	GAS METER		W WATER
	CABLE TV JUNCTION BOX		FENCE LINE
	WATER METER		



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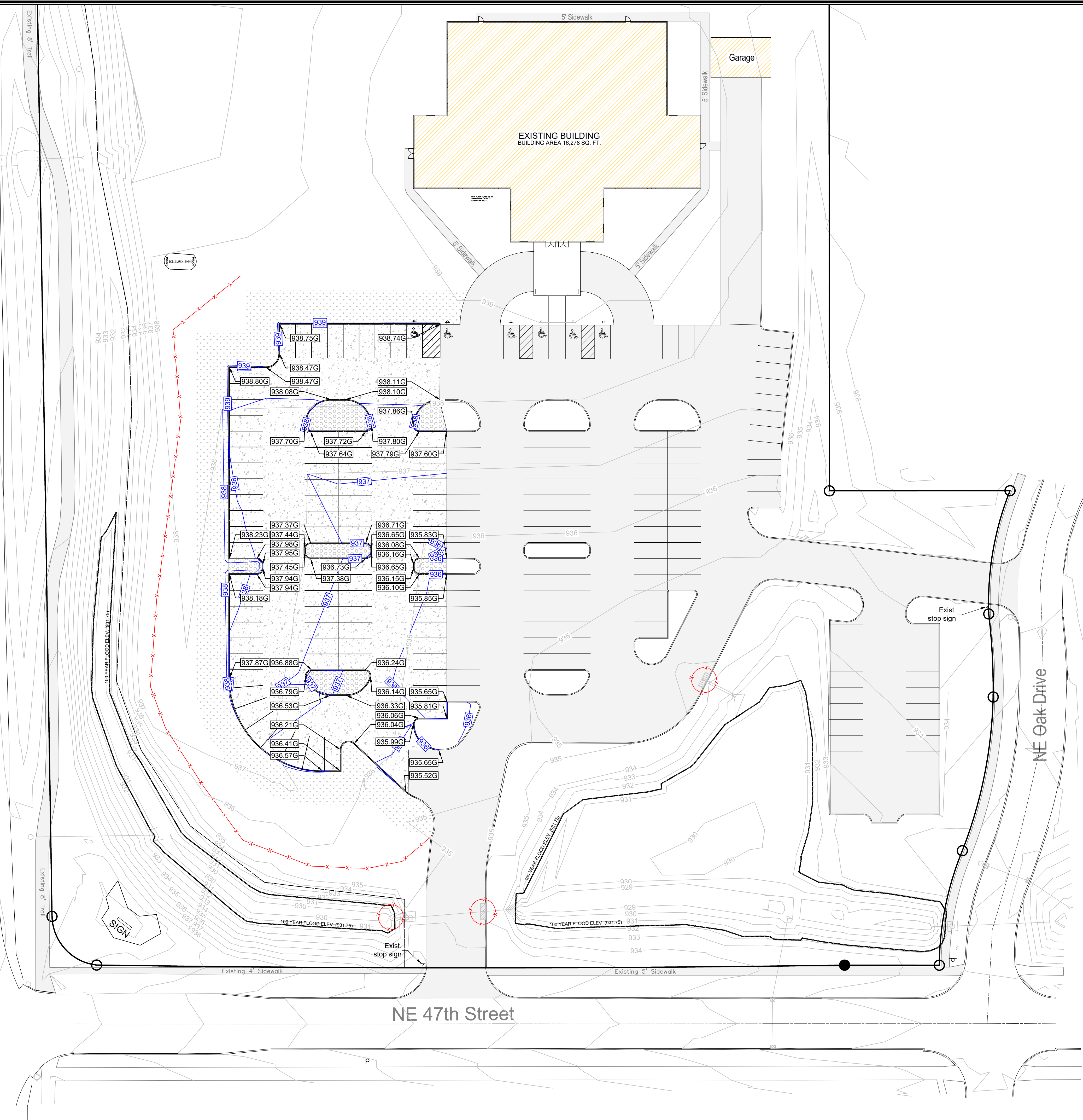
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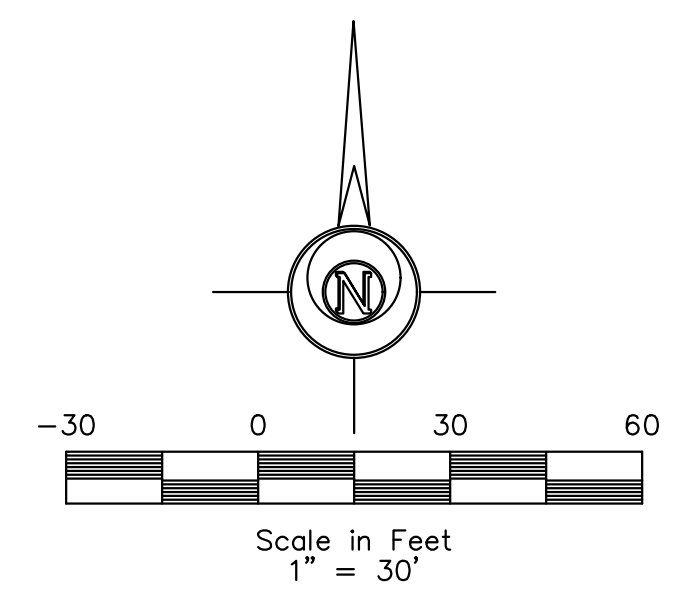
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		DRAWING NO: 24-039	

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N. Ankeny Blvd. (US Highway 69)



- PROPOSED 6" MIN. STANDARD PAVEMENT
30,054 SQ. FT.
- PROPOSED SOD/SEED
11,202 SQ. FT.
- PROPOSED LANDSCAPING ROCK
3,271 SQ. FT.
- EXISTING PAVEMENT
79,082 SQ. FT.
- EXISTING BUILDING
- FILTER SOCK



LEGEND:

+/-	MORE OR LESS	⊙	ELEC. MANHOLE
F.F.	FINISHED FLOOR	⊙	SANITARY SEWER MANHOLE
123.45G	GUTTER ELEVATION	⊙	STORM SEWER MANHOLE
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⊙	LIGHT POLE	⊙	CATV — CABLE TELEVISION
⊙	AREA LIGHT	⊙	FO — FIBER OPTIC
⊙	GUY ANCHOR	⊙	GAS — GAS LINE
⊙	UTILITY PEDESTAL	⊙	OHE — OVERHEAD ELEC.
⊙	ELEC. TRANSFORMER	⊙	OHT — OVERHEAD TEL.
⊙	ELEC. METER	⊙	SAN — SANITARY SEWER
⊙	ELEC. BOX	⊙	ST — STORM SEWER
⊙	ELEC. VAULT	⊙	UGE — UNDERGROUND ELEC.
⊙	FIBER OPTIC VAULT	⊙	UGT — UNDERGROUND TEL.
⊙	GAS METER	⊙	W — WATER
⊙	CABLE TV JUNCTION BOX	⊙	— — — — — FENCE LINE
⊙	WATER METER		

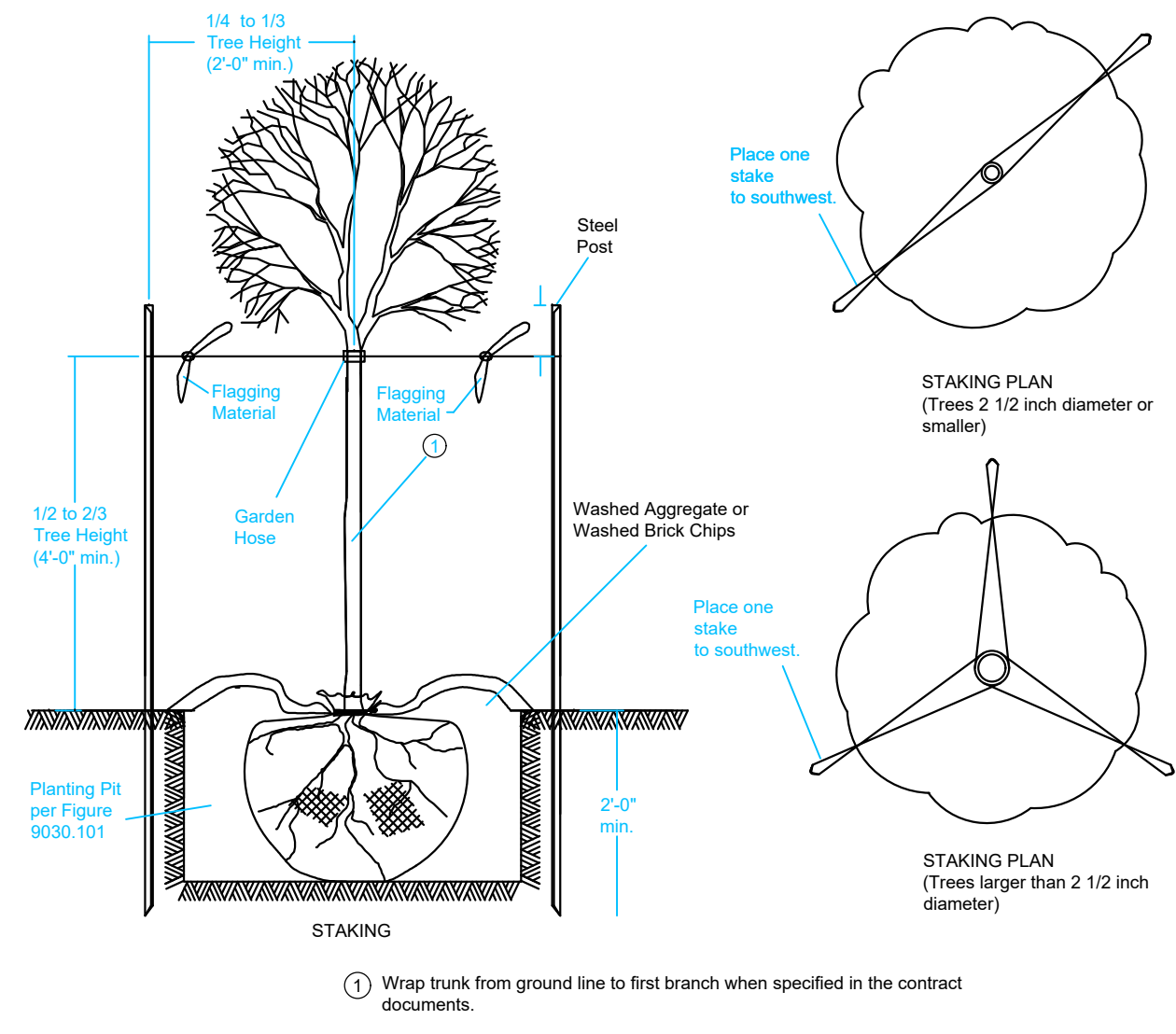
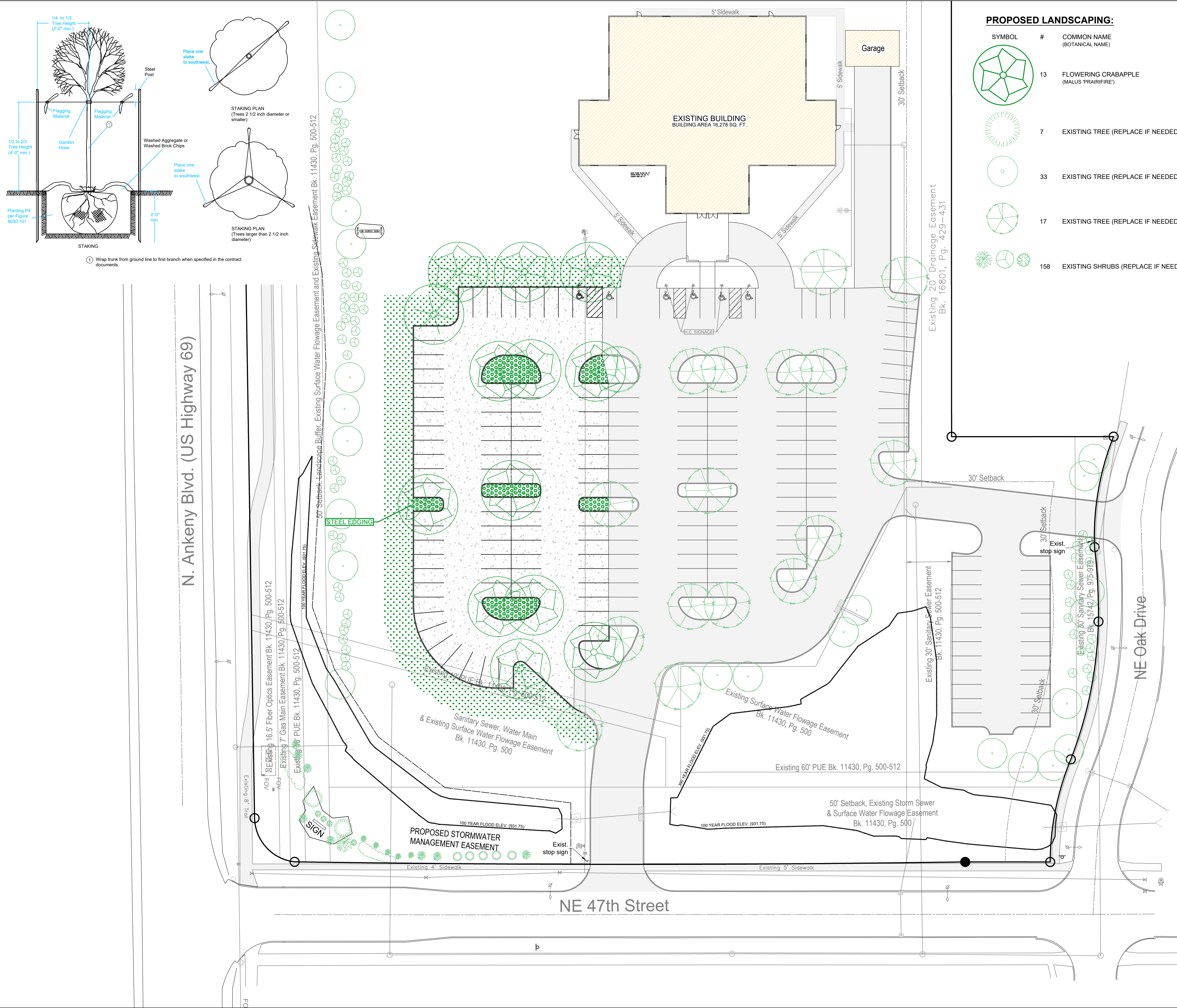
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COMMUNITY BAPTIST CHURCH
 4703 N ANKENY BLVD
 ANKENY, IA

PRELIMINARY

DATE: 06.25.2024
 DRAWN BY: L. BURKE
 SCALE: 1"=30'
 SHEET NO: 24-039

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 C-201 - GRADING SHEET



PROPOSED LANDSCAPING:

SYMBOL	#	COMMON NAME (BOTANICAL NAME)	MINIMUM PLANTING SIZE	MATURE SIZE (HEIGHT & SPREAD)
	13	FLOWERING CRABAPPLE (MALUS 'PRAIRIFIRE')	5' AT TIME OF PLANTING	20' & 15'
	7	EXISTING TREE (REPLACE IF NEEDED)		
	33	EXISTING TREE (REPLACE IF NEEDED)		
	17	EXISTING TREE (REPLACE IF NEEDED)		
	158	EXISTING SHRUBS (REPLACE IF NEEDED)		

LANDSCAPING REQUIREMENTS:

ONE LANDSCAPE UNIT PER 3,000 SQ. FT. OF OPEN SPACE WITH ONE LANDSCAPE UNIT CONSISTING OF TWO TREES AND SIX SHRUBS OR THREE TREES AND THREE SHRUBS.

401,165 GROSS LAND AREA x .20 = 80,233 MIN. OPEN SPACE
 80,233 MIN. OPEN SPACE / 3,000 = 27 LANDSCAPE UNITS
 27 LANDSCAPE UNITS x 2 = 54 TREES REQUIRED
 27 LANDSCAPE UNITS x 6 = 162 SHRUBS REQUIRED
 OR
 27 LANDSCAPE UNITS x 3 = 81 TREES REQUIRED
 27 LANDSCAPE UNITS x 3 = 81 SHRUBS REQUIRED

TOTAL TREES PROVIDED = 70
 TOTAL SHRUBS PROVIDED = 158

SHADING TREE CALCULATION:

GROSS PAVED AREA x .20 = REQUIRED SHADED AREA
 REQUIRED SHADED AREA / 706 SQ. FT. = SHADING TREES REQUIRED.

109,136 x .20 = 21,827 / 706 = 31 SHADING TREES REQUIRED.
 63 SHADING TREES PROVIDED.

- PROPOSED 6" MIN. STANDARD PAVEMENT
30,054 SQ. FT.
- PROPOSED SOD/SEED
11,202 SQ. FT.
- PROPOSED LANDSCAPING ROCK
3,271 SQ. FT.
- EXISTING PAVEMENT
79,082 SQ. FT.
- EXISTING BUILDING

LEGEND:

- +/- MORE OR LESS
- F.F. FINISHED FLOOR
- 123.45G GUTTER ELEVATION
- 123.45TC TOP OF CURB ELEVATION
- 123.45/123.45 EXISTING/ PROPOSED ELEVATION
- FL FLOWLINE ELEVATION
- CONTROL POINT
- CALCULATED CORNER
- FOUND CORNER
- CALCULATED SECTION CORNER
- FOUND SECTION CORNER
- ▲ PARKING SPACE
- ▲ SIGN
- STREET LIGHT
- POWER POLE
- LIGHT POLE
- AREA LIGHT
- GUY ANCHOR
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- WATER METER
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- SANITARY SEWER MANHOLE
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- TRAFFIC MANHOLE
- CLEANOUT
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PRELIMINARY

DATE: 06.25.2024 DRAWING NO: 24-039
 SCALE: 1"=30'

C-701 - LANDSCAPING SHEET



PLAN AND ZONING COMMISSION
September 17, 2024
6 : 30 PM

Print

ORIGINATING DEPARTMENT:
Planning and Building

COUNCIL GOAL:
Enhance Quality of Life

ACTION REQUESTED:
Motion

LEGAL:

SUBJECT:

Deer Creek Rowhomes Plat 1 Final Plat

EXECUTIVE SUMMARY:

See attached staff report.

FISCAL IMPACT: **No**

STAFF RECOMMENDATIONS:

PREVIOUS COUNCIL/COMMISSION/BOARD ACTION(S):

PUBLIC OUTREACH EFFORTS:

ACTION REQUESTED:

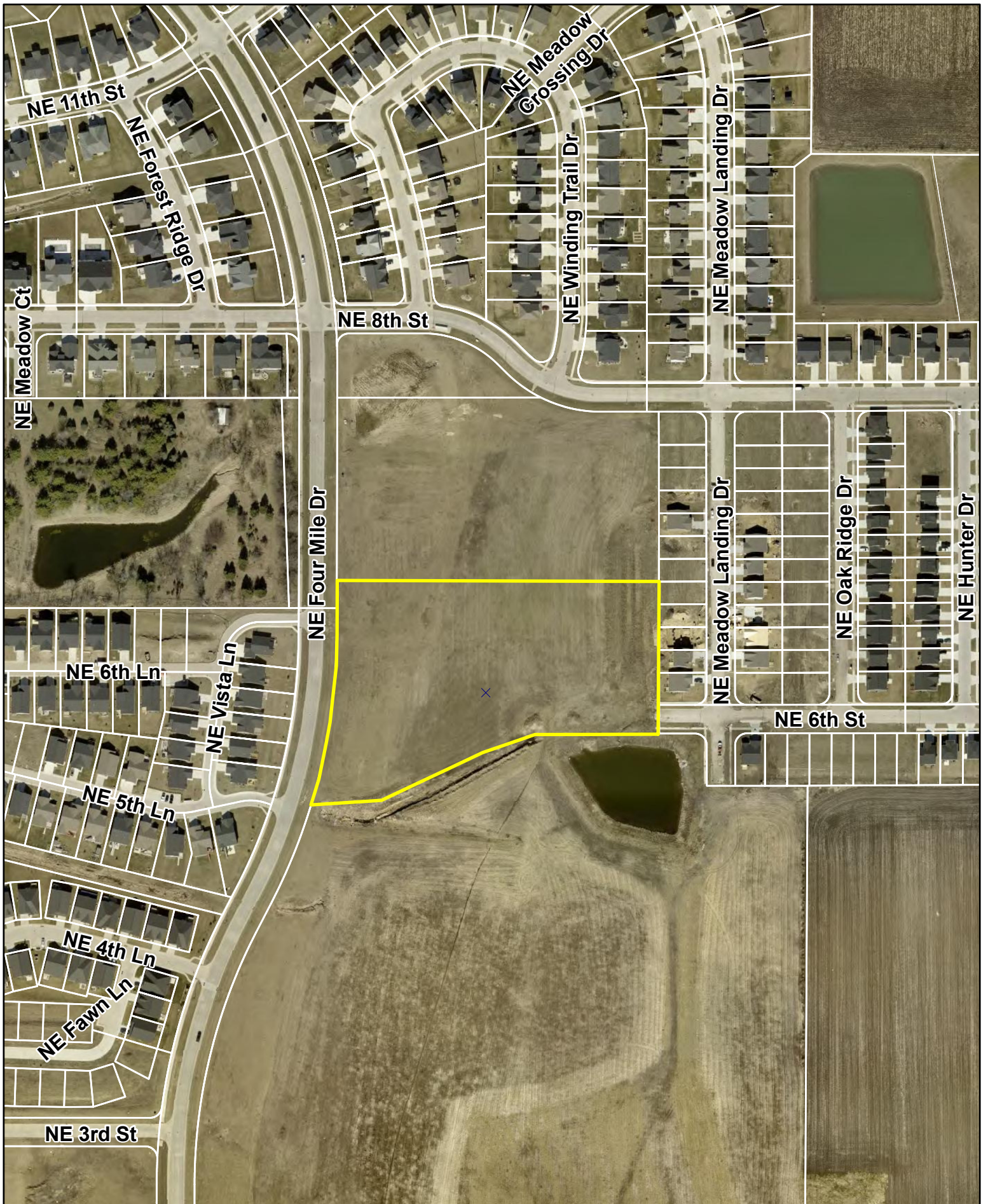
Action: Consider MOTION to recommend City Council approval of Deer Creek Rowhomes Plat 1 Final Plat.

ADDITIONAL INFORMATION:

ATTACHMENTS:

Click to download

Aerial Map
Staff Report
Applicant Letter
Final Plat



N



1 inch = 257 feet

Date: 9/11/2024

Deer Creek Rowhomes Aerial Map



Agenda Item: Deer Creek Rowhomes Plat 1 – Final Plat
Report Date: September 12, 2024
Prepared by: Ryan Kirschman *EJC*
Planner I

Staff Recommendation:

That the Plan and Zoning Commission recommend City Council approval of the Deer Creek Rowhomes Plat 1, Final Plat.

Project Summary:

Deer Creek Rowhomes Plat 1 is a proposed 5.69-acre (+/-) development, located north of E 1st Street and east of NE Four Mile Drive. The property is zoned PUD, Deer Creek PUD. The subject plat consists of creating 60 buildable lots anticipated to be utilized for rowhomes, and two outlots that will be maintained by the homeowner’s association. Outlot Y will be used for storm water management and Outlot Z identified as ‘community space’ in the plat drawings, this space will be NE 7th Lane.

Project Report:

- Streets:** The primary access to the development off NE 6th Street will be from NE Winding Trail Lane. NE 7th Lane will cross NE Winding Trail Lane and to the east will give access to NE Darby Lane and to the west NE Hart Lane. The streets within the plat will be private streets maintained by the homeowner’s association.
- Water:** The applicant is proposing to connect to the water main that runs along NE 6th Street. The site will be serviced with eight-inch water main throughout the site.
- Sewer:** The applicant is proposing to connect to the sewer line stub that is located on the western portion of the site. The site will be serviced with eight-inch sewer lines.
- Drainage:** The proposed site will drain towards the southeast corner of the proposed development where Outlot Y will serve as a stormwater management facility. This plat lies in the Middle Four Mile Creek watershed and all storm water from the site will ultimately flow into Four Mile Creek.
- Parkland:** Parkland within the Deer Creek PUD includes a 6.61-acre park located on Outlot W, The Crossings at Deer Creek Plat 12, which was deeded to the City on January 10, 2023. In order to fulfill the remaining parkland dedication requirement as specified in the City’s Subdivision Regulations in Chapter 200.35.A.1 (Criteria for Requiring Parkland Dedication) for development, the developer requests that the City accept payment in lieu of parkland for the subject plat, in an amount equal to \$6,828 based on \$12,000 per acre.

**CITY OF ANKENY
SUBDIVISION DATA**

NAME OF PLAT: Deer Creek Rowhomes Plat 1
NAME OF OWNER/ DEVELOPER: Deer Creek Estates, LLC / Jerry's Homes

GENERAL INFORMATION:

PLAT LOCATION: East of NE Four Mile Drive and north of E 1st Street
SIZE OF PLAT: 5.69 acres
ZONING: Deer Creek PUD

LOTS:

NUMBER: 60 buildable lots, 1 Outlot for storm water management, 1 Outlot for street dedication
SIZE/DENSITY: 10.54 du/acre
USE: Residential
BUILDING LINES: Parcel P
Front setback: 25 feet from Four Mile Drive
15 feet from internal public right of ways
Rear setback: 30 feet where abutting residential, civic, or park land use

PARK SITE DEDICATION:

Parkland within the Deer Creek PUD includes a 6.61-acre park located on Outlot W, The Crossings at Deer Creek Plat 12, which was deeded to the City on January 10, 2023. In order to fulfill the remaining parkland dedication requirement as specified in the City's Subdivision Regulations in Chapter 200.35.A.1 (Criteria for Requiring Parkland Dedication) for development, the developer requests that the City accept payment in lieu of parkland for the subject plat, in an amount equal to \$6,828 based on \$12,000 per acre.

ADJACENT LANDS:

NORTH: Undeveloped, Deer Creek PUD
SOUTH: Undeveloped, Deer Creek PUD
EAST: The Crossings at Deer Creek Plat 14, Deer Creek PUD
WEST: Deer Creek South Townhomes Plat 2, Deer Creek PUD

WASTE WATER:

PROJECTED FLOWS: 5.69 acres of land X 3000 gal. per day/acre of developable land = 17,070 GPD

WRA CAPACITY: 11.2 MGD; current daily avg. 4.4 MGD

NEAREST DOWNSTREAM INTERCEPTOR/TRUNK LINE: Rock Creek Trunk Sewer

STORM WATER:

BASIN FLOWS: This plat lies in the Middle Four Mile Creek Basin.

WATER SYSTEM:

USAGE: 5.69 acres of land X 1000 gal. per day/acre of developable land = 5,690 GPD

SUPPLY CAPACITY: 13.72 MGD; Current daily avg. 6.2 MGD

FINAL PLAT DRAWING:

Staff recommends approval.

CONSTRUCTION PLANS:

To be approved.

August 23, 2024

Honorable Mayor, City Council and Plan and Zoning Commission Members
c/o Ryan Kirschman
City of Ankeny
1210 NW Prairie Ridge Dr.
Ankeny, Iowa 50023

RE: Deer Creek Rowhomes Plat 1
Site Plan, Final Plat and Construction Drawing Submittal


Dear Plan and Zoning Commission Members, Council Members, and Honorable Mayor;

On behalf of Jerry's Homes Inc., we are pleased to submit herewith a site plan, final plat, and construction drawings for Deer Creek Rowhomes Plat 1 located within the Deer Creek PUD, east of NE Four Mile Drive, north of NE 6th Street. The property contains approximately 10.98 acres.

Plat 1 will include development of 60 3-, 5-, and 6-plex rowhomes, which is consistent with the Deer Creek PUD. The project contains several new private streets which connect to NE 6th Street and NE 8th Street. Public sanitary sewer and water main will be extended from the public streets to provide service for the development. Storm water detention will be provided in a new dry-bottom detention basin in the SW corner of the property and in an existing regional detention basin located northwest of the site.

We are requesting that preliminary approval of the final plat be granted by City Council at the next regular City Council meeting.

Sincerely,
CIVIL DESIGN ADVANTAGE, LLC

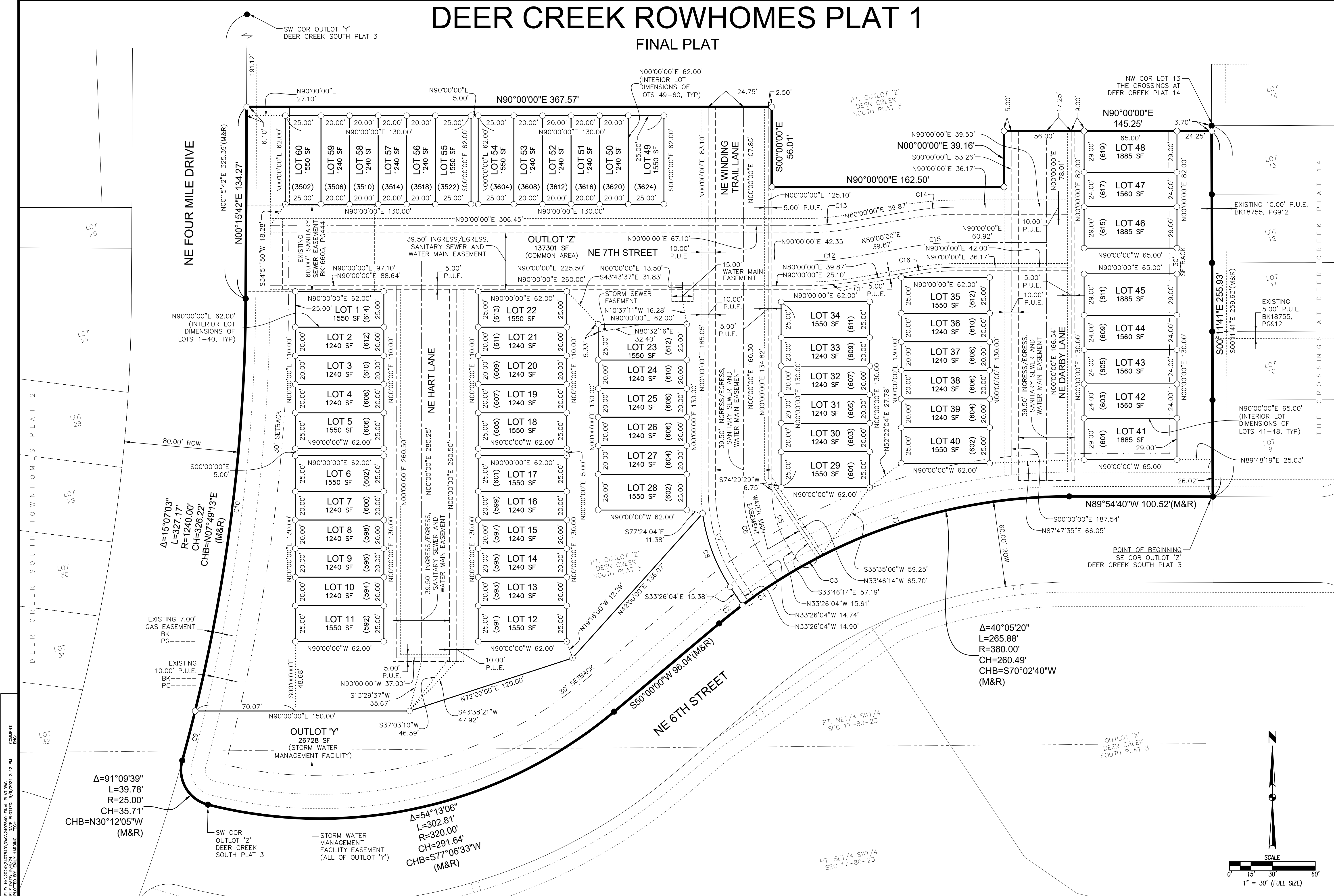


Emily Harding, Project Manager

cc: Jerry's Homes Inc.

DEER CREEK ROWHOMES PLAT 1

FINAL PLAT



FILE: H:\2024\2407540\DWG\2407540-FINAL PLATING DEER CREEK ROWHOMES PLAT 1.DWG
 PLOTTED BY: SALLY HARRISON
 DATE: 9/16/2024 2:42 PM

DATE	REVISIONS
08/02/24	
09/06/24	THIRD SUBMITTAL
09/26/24	SECOND SUBMITTAL
08/02/24	FIRST SUBMITTAL

4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 369-4400



CIVIL DESIGN ADVANTAGE
 ANKENY, IOWA

DEER CREEK ROWHOMES PLAT 1
FINAL PLAT

2407.540
 2/2

ENGINEER:	TECH:	REVIEW:



PLAN AND ZONING COMMISSION
September 17, 2024
6 : 30 PM

Print

ORIGINATING DEPARTMENT:
Planning and Building

COUNCIL GOAL:
Enhance Quality of Life

ACTION REQUESTED:
Motion

LEGAL:

SUBJECT:

Deer Creek Rowhomes Site Plan

EXECUTIVE SUMMARY:

See attached staff report.

FISCAL IMPACT: **No**

STAFF RECOMMENDATIONS:

PREVIOUS COUNCIL/COMMISSION/BOARD ACTION(S):

PUBLIC OUTREACH EFFORTS:

ACTION REQUESTED:

Action: Consider MOTION to approve the site plan for Deer Creek Rowhomes, subject to recordation of the final plat.

ADDITIONAL INFORMATION:

ATTACHMENTS:

Click to download

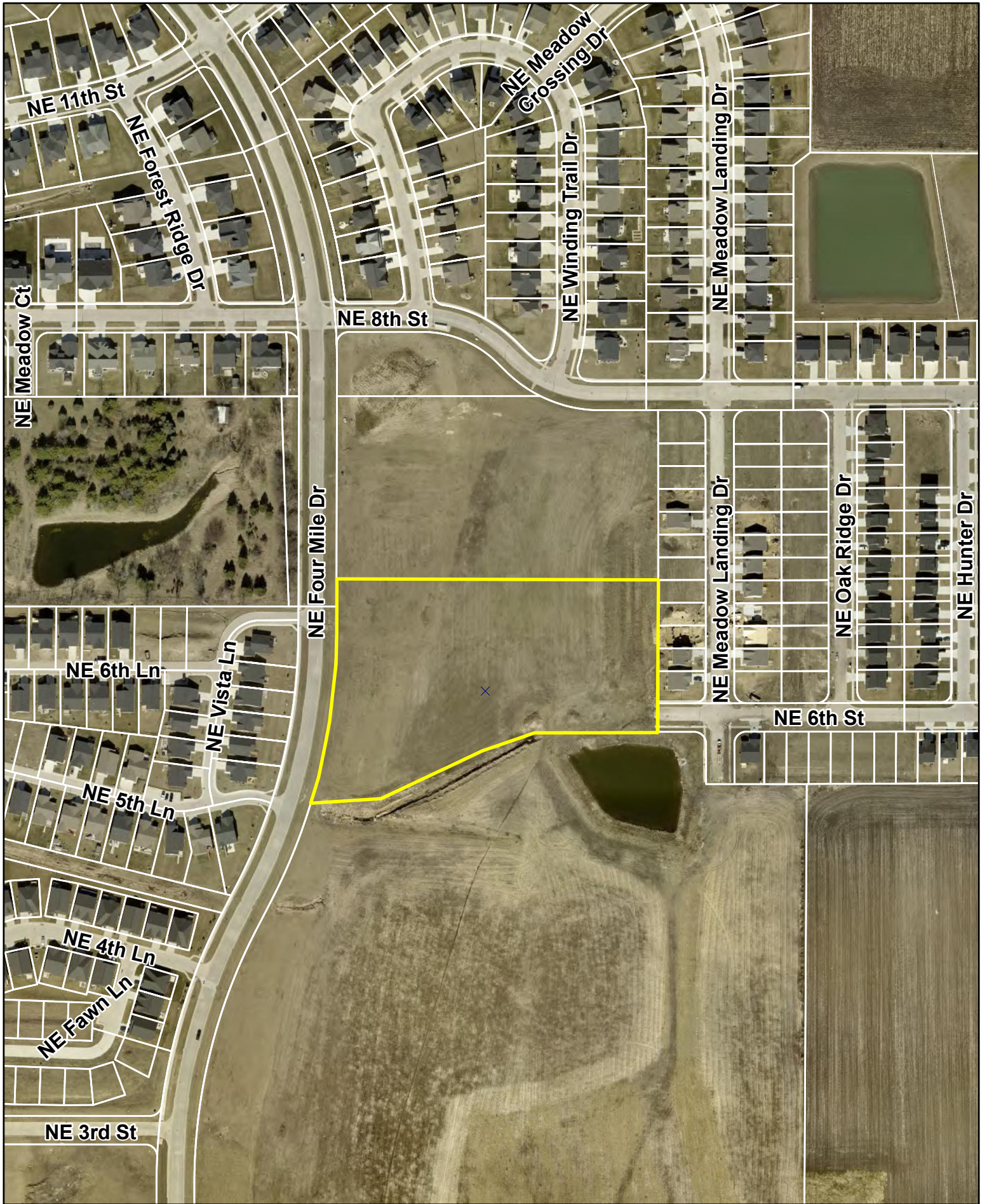
[Aerial Map](#)

[Staff Report](#)

[Applicant Letter](#)

[Site Plan](#)

[Elevations](#)



N



1 inch = 257 feet

Date: 9/11/2024

Deer Creek Rowhomes Aerial Map



Plan and Zoning Commission
Staff Report

Meeting Date: September 17, 2024

Agenda Item: Deer Creek Rowhomes – Site Plan
Report Date: September 12, 2024
Prepared by: Ryan Kirschman
Planner I

ESL

Staff Recommendation:

That the Plan and Zoning Commission approve the site plan for Deer Creek Rowhomes subject to recordation of the final plat.

Project Summary:

The applicant, Jerry's Homes, is proposing a two phased project of 124 residential units in three-plex, five-plex, or six-plex layouts. Phase 1 of the project will be on the south portion of the site and consist of 60 residential units. Phase 2 of the project will be on the north portion of the site and consist of the remaining 64 residential units. The site is located east of NE Four Mile Drive and north of E 1st Street. The property is zoned PUD, Deer Creek PUD and the surrounding properties are also part of the Deer Creek PUD. The future land use for the property is Community Commercial per The Ankeny Plan 2040 Figure 12.10 Ankeny Future Land Use Map.

The proposed site will have two access points, one from the north via NE 8th Street and one from the south via NE 6th Street. During Phase 1 the only access to the property will be via NE 6th Street. All of the streets included in the site plan will be privately owned and maintained by the Homeowner's association. The site plan will have adequate parking with 33 stalls being proposed and 31 stalls are required.

The utilities servicing the site will all be located underground. The site will connect to water main on the south side of the property along NE 6th Street and will have eight-inch water main servicing the site. The site will connect to a sanitary line stub that is located on the western portion of the site and eight-inch sanitary line will be installed to service the property. Storm water on the site will be addressed with a dry bottom detention pond on the southwest corner of the property in addition to utilizing the existing basin on the northwest corner of the site.

A landscaping plan has been included as part of the site plan. Screening has been provided along the north property line adjacent to future properties. Headlight screening adjacent to the open parking stalls has also been included. All other open space and landscaping requirements have been met. Additionally, the applicant has requested payment in lieu of a park site dedication. This payment will be required before Council submission of the final plat.

August 23, 2024

Honorable Mayor, City Council and Plan and Zoning Commission Members
c/o Ryan Kirschman
City of Ankeny
1210 NW Prairie Ridge Dr.
Ankeny, Iowa 50023

RE: Deer Creek Rowhomes Plat 1
Site Plan, Final Plat and Construction Drawing Submittal

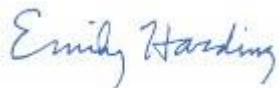
Dear Plan and Zoning Commission Members, Council Members, and Honorable Mayor;

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We are requesting that preliminary approval of the final plat be granted by City Council at the next regular City Council meeting.

Sincerely,
CIVIL DESIGN ADVANTAGE, LLC



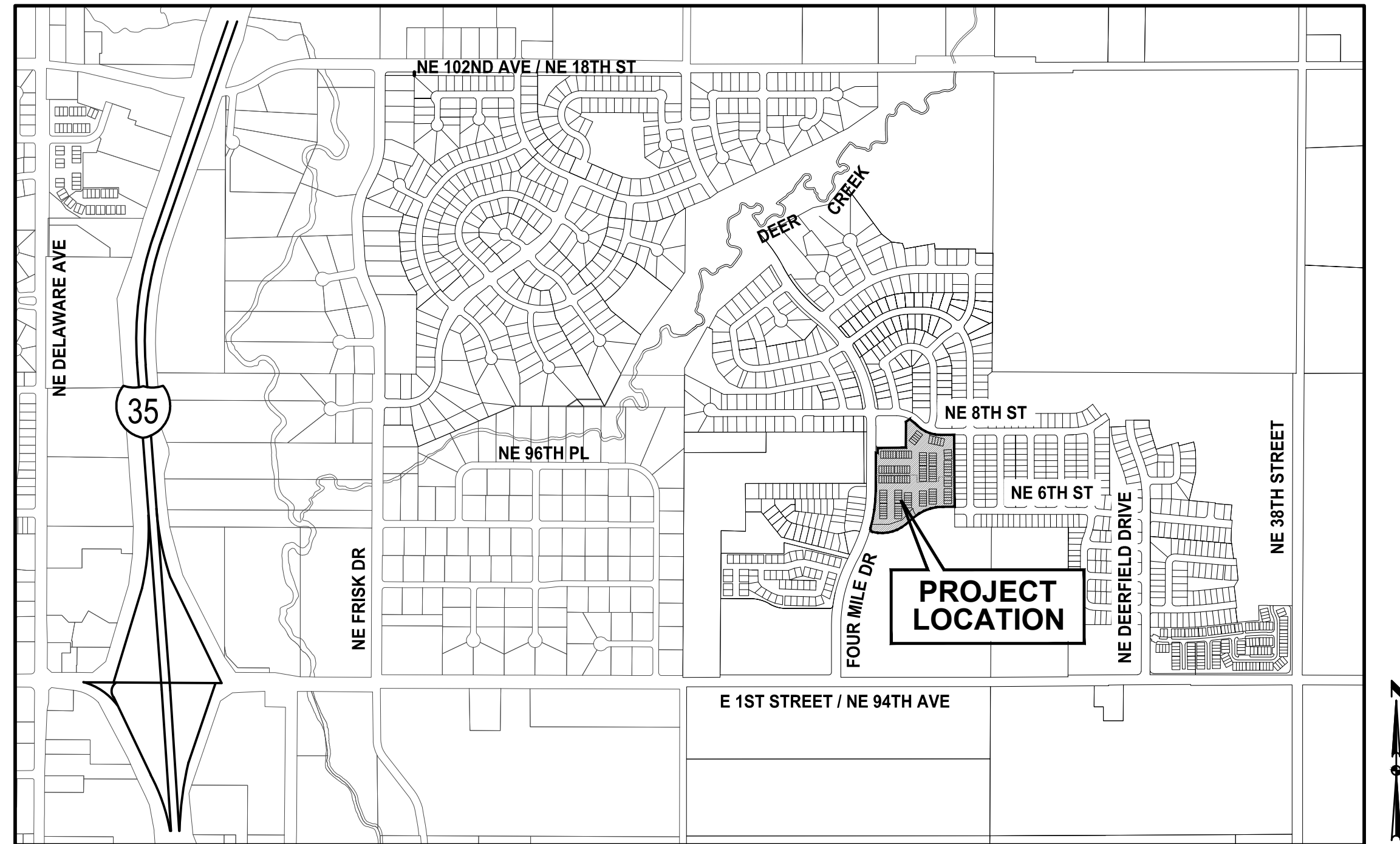
Emily Harding, Project Manager

cc: Jerry's Homes Inc.

SITE PLAN FOR: DEER CREEK ROWHOMES

ANKENY, IOWA

VICINITY MAP NOT TO SCALE



ANKENY, IOWA

INDEX OF SHEETS

NO.	DESCRIPTION
1.0	COVER SHEET
2.0-2.1	DETAILS
3.0-3.2	DIMENSION PLAN
4.0-4.4	GRADING PLAN
5.0-5.1	UTILITY PLAN
6.0-6.1	LANDSCAPE PLAN
7.0	EROSION AND SEDIMENT CONTROL PLAN

GENERAL LEGEND

PROPOSED	EXISTING
PROPERTY BOUNDARY	SANITARY MANHOLE
SECTION LINE	WATER VALVE BOX
CENTER LINE	FIRE HYDRANT
RIGHT OF WAY	WATER CURB STOP
BUILDING SETBACK	WELL
PERMANENT EASEMENT	STORM SEWER MANHOLE
TEMPORARY EASEMENT	STORM SEWER SINGLE INTAKE
TYPE SW-501 STORM INTAKE	STORM SEWER DOUBLE INTAKE
TYPE SW-502 STORM INTAKE	FLARED END SECTION
TYPE SW-503 STORM INTAKE	DECIDUOUS TREE
TYPE SW-505 STORM INTAKE	CONIFEROUS TREE
TYPE SW-506 STORM INTAKE	DECIDUOUS SHRUB
TYPE SW-512 STORM INTAKE	CONIFEROUS SHRUB
TYPE SW-513 STORM INTAKE	ELECTRIC POWER POLE
TYPE SW-401 STORM MANHOLE	GUY ANCHOR
TYPE SW-402 STORM MANHOLE	STREET LIGHT
FLARED END SECTION	POWER POLE W/ TRANSFORMER
TYPE SW-301 SANITARY MANHOLE	UTILITY POLE W/ LIGHT
STORM/SANITARY CLEANOUT	ELECTRIC BOX
WATER VALVE	ELECTRIC TRANSFORMER
FIRE HYDRANT ASSEMBLY	ELECTRIC MANHOLE OR VAULT
SIGN	TRAFFIC SIGN
DETECTABLE WARNING PANEL	TELEPHONE JUNCTION BOX
WATER CURB STOP	TELEPHONE MANHOLE/VAULT
SANITARY SEWER	TELEPHONE POLE
SANITARY SERVICE	GAS VALVE BOX
STORM SEWER	CABLE TV JUNCTION BOX
STORM SERVICE	CABLE TV MANHOLE/VAULT
WATERMAIN WITH SIZE	MAIL BOX
WATER SERVICE	BENCHMARK
SAWCUT (FULL DEPTH)	SOIL BORING
SILT FENCE	UNDERGROUND TV CABLE
USE AS CONSTRUCTED	GAS MAIN
MINIMUM PROTECTION ELEVATION	FIBER OPTIC
	UNDERGROUND TELEPHONE
	OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	FIELD TILE
	SANITARY SEWER W/ SIZE
	STORM SEWER W/ SIZE
	WATER MAIN W/ SIZE

OWNER

DEER CREEK ESTATES, LLC
160 ADVENTURELAND DRIVE NW
ALTOONA, IA 50009
CONTACT: JEFF GRUBB
PH: (515) 208-5227

APPLICANT

JERRY'S HOMES INC.
3900 WESTOWN PARKWAY, SUITE 100
WEST DES MOINES, IA 50266
CONTACT: JENNIFER ELLISON
EMAIL: JENNIFERE@JERRYSHOMES.COM
PH: (515) 727-0356

ENGINEER

CIVIL DESIGN ADVANTAGE
4121 NW URBANDALE DRIVE
URBANDALE, IA 50322
CONTACT: EMILY HARDING
EMAIL: EMILYH@CDA-ENG.COM
PH: (515) 369-4400

SURVEYOR

CIVIL DESIGN ADVANTAGE
4121 NW URBANDALE DRIVE
URBANDALE, IA 50322
CONTACT: CHARLIE MCGLOTHLEN
EMAIL: CHARLIEM@CDA-ENG.COM
PH: (515) 369-4400

SUBMITTAL DATES

FIRST SUBMITTAL: 08/02/2024
SECOND SUBMITTAL: 08/23/2024
THIRD SUBMITTAL: 09/06/2024

LEGAL DESCRIPTION

OUTLOT Z, DEER CREEK SOUTH PLAT 3

ZONING

DEER CREEK PUD

PROPOSED USE

MULTI-FAMILY RESIDENTIAL

DEVELOPMENT SUMMARY

AREA: 10.98 ACRES (478,323 SF)

SETBACKS:

FRONT: 25 FEET FROM FOURMILE DRIVE
15 FEET FROM INTERNAL PUBLIC RIGHT OF WAYS
REAR: 30 FEET WHERE ABUTTING RESIDENTIAL

OPEN SPACE CALCULATION:

BUILDING = 129,422 SF
PAVEMENT = 71,332 SF
DRIVEWAYS = 40,252 SF
SIDEWALK = 35,606 SF
IMPERVIOUS = 276,612 SF
OPEN SPACE PROVIDED = 201,711 SF (42%)

UNITS:

124 UNITS (11.29 UNITS PER ACRE)

PARKING:

TOTAL REQUIRED (1 PER 4 UNITS) = 31 STALLS
TOTAL PROVIDED = 33 STALLS

BENCHMARKS

- FND 1" DISC AT THE SW COR OF SEC 17-80-23.
ELEVATION=948.37
- FND 1" DISC AT THE S1/4 COR OF SEC 17-80-23.
ELEVATION=972.44



UTILITY WARNING

ANY UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY AND RECORDS OBTAINED BY THIS SURVEYOR. THE SURVEYOR MAKES NO GUARANTEE THAT THE UTILITIES SHOWN COMPRISE ALL THE UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UTILITIES SHOWN ARE IN THE EXACT LOCATION SHOWN.



4121 NW URBANDALE DRIVE, URBANDALE, IA 50322

PH: (515) 369-4400

PROJECT NO. 2407.540

REFER TO CONSTRUCTION MATERIALS TESTING GEOTECHNICAL EXPLORATION REPORT (PROJECT NO. 242054DCS, DATED 07/09/2024) FOR GEOTECHNICAL REQUIREMENTS.

THE PROJECT REQUIRES AN IOWA NPDES PERMIT #2 AND CITY OF ANKENY COSESCO PERMIT. CIVIL DESIGN ADVANTAGE WILL PROVIDE THE PERMITS AND THE INITIAL STORM WATER POLLUTION PREVENTION PLAN (SWPPP) FOR THE CONTRACTORS USE DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPDATING THE SWPPP THROUGHOUT CONSTRUCTION AND MEETING LOCAL, STATE AND FEDERAL REQUIREMENTS.

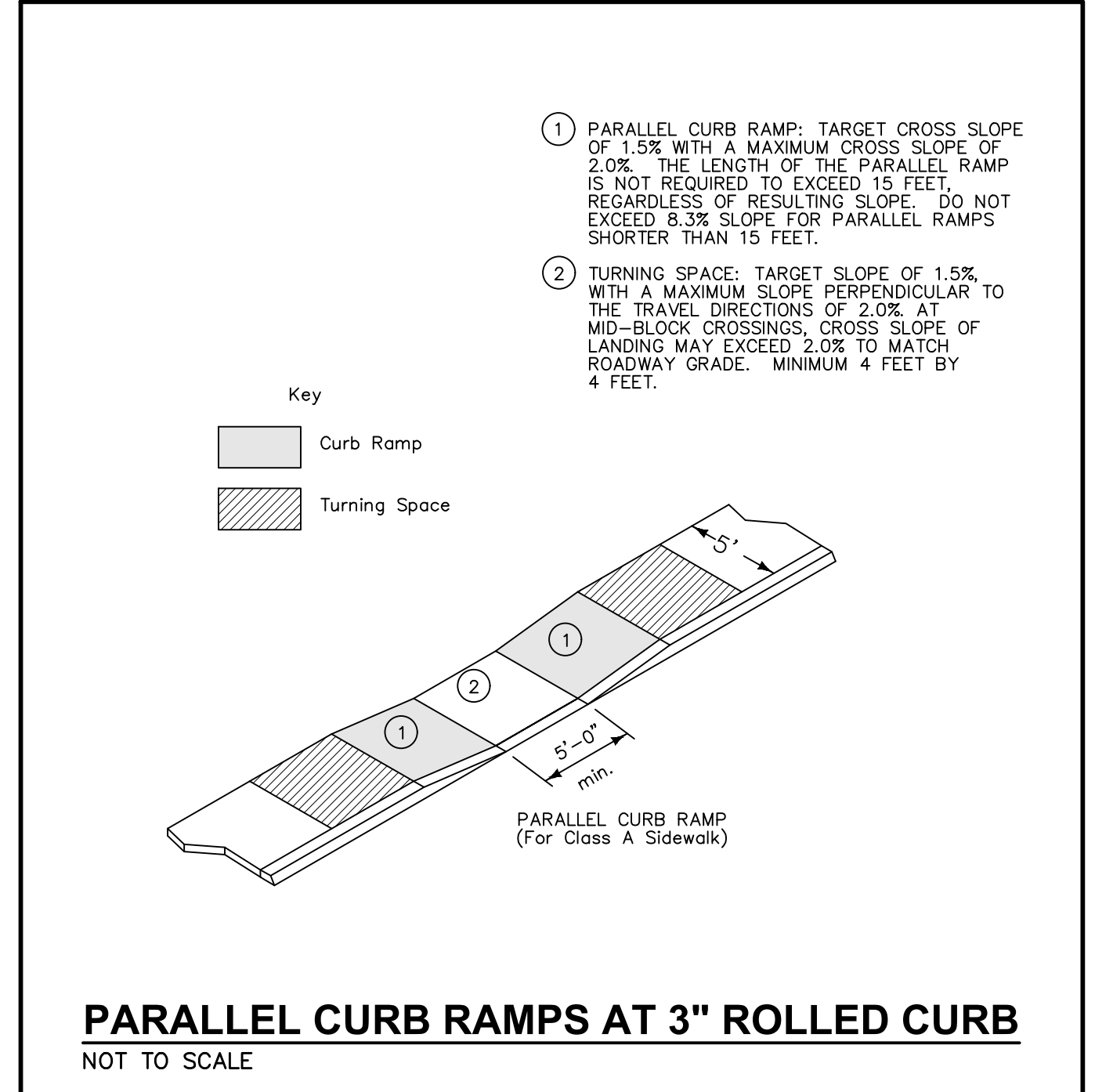
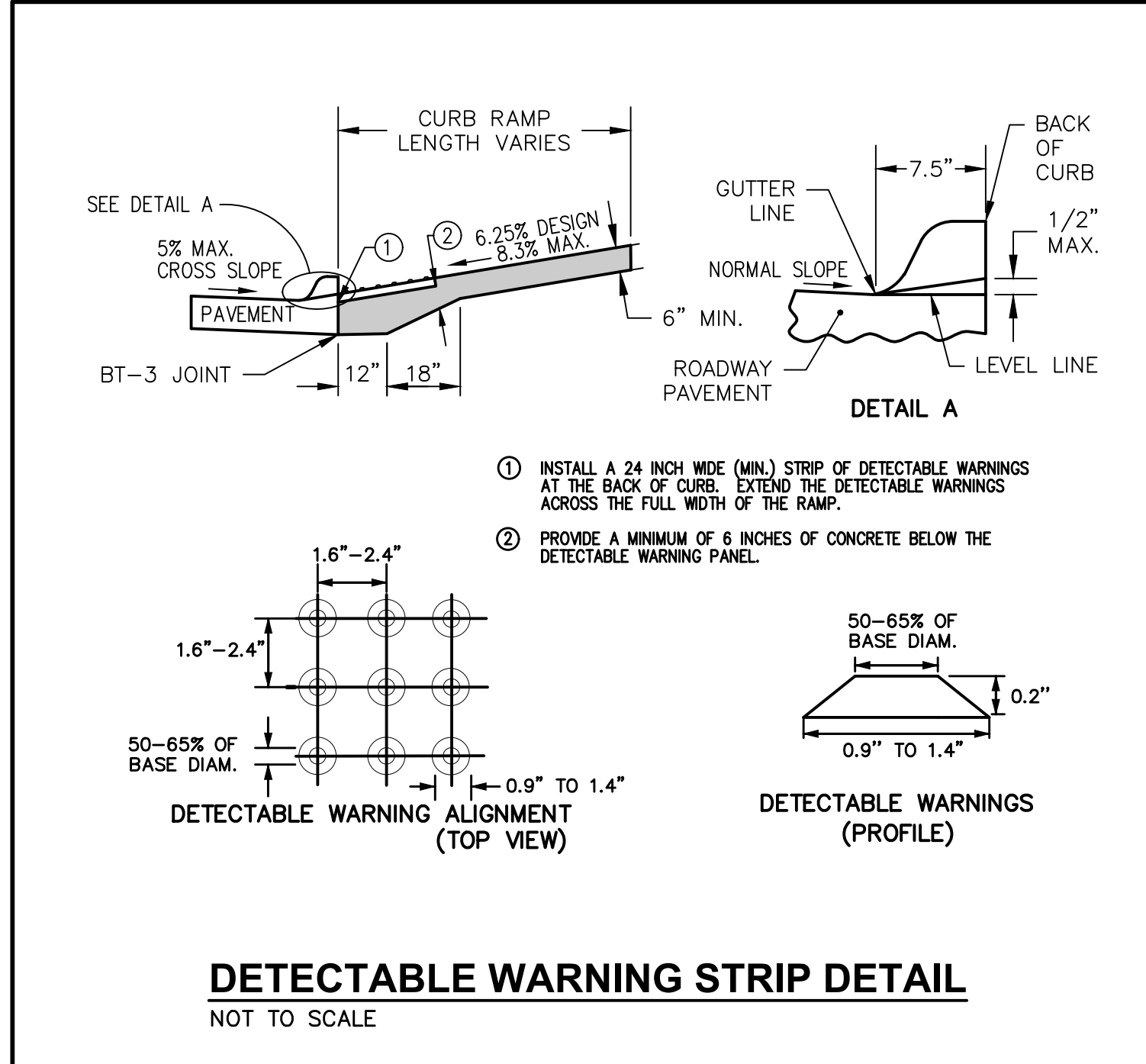
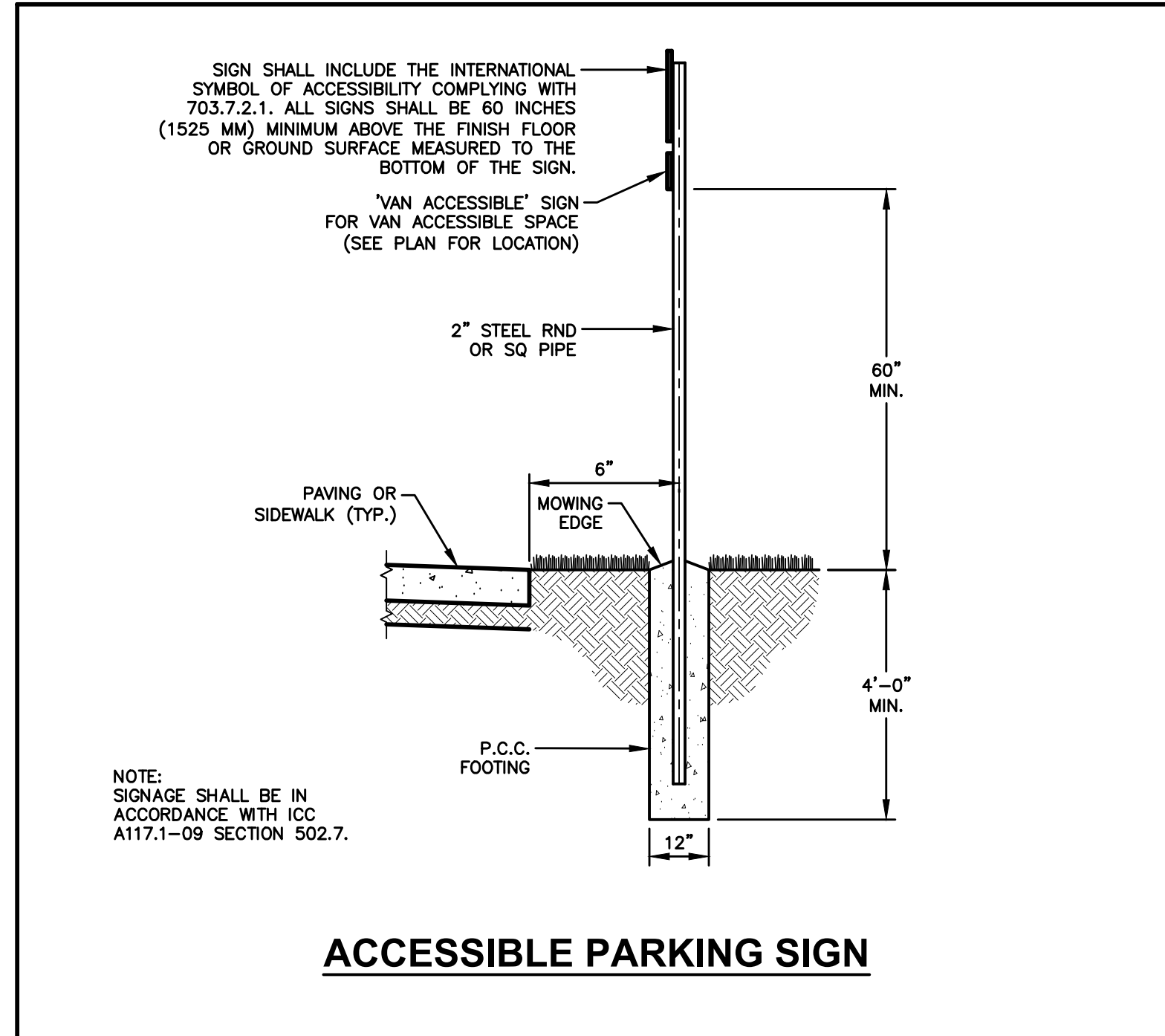
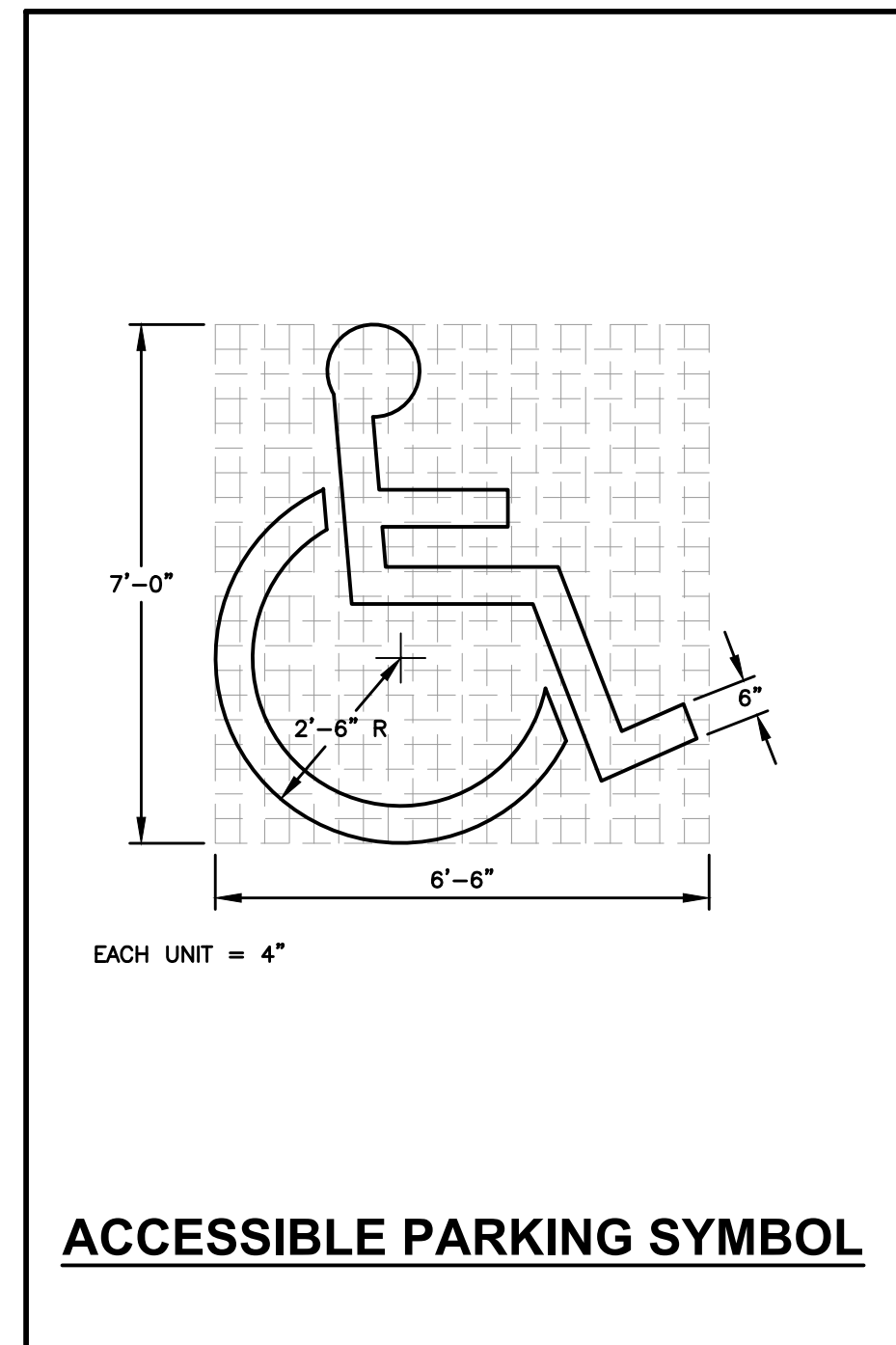
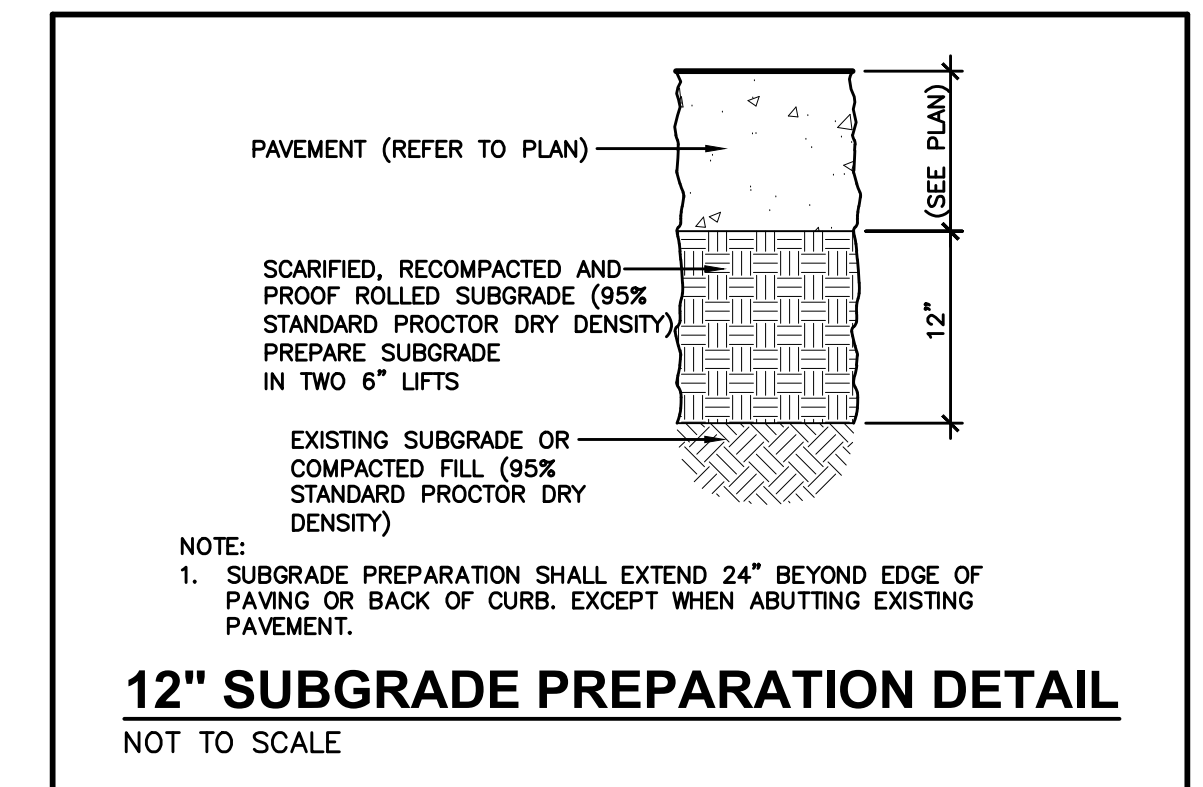
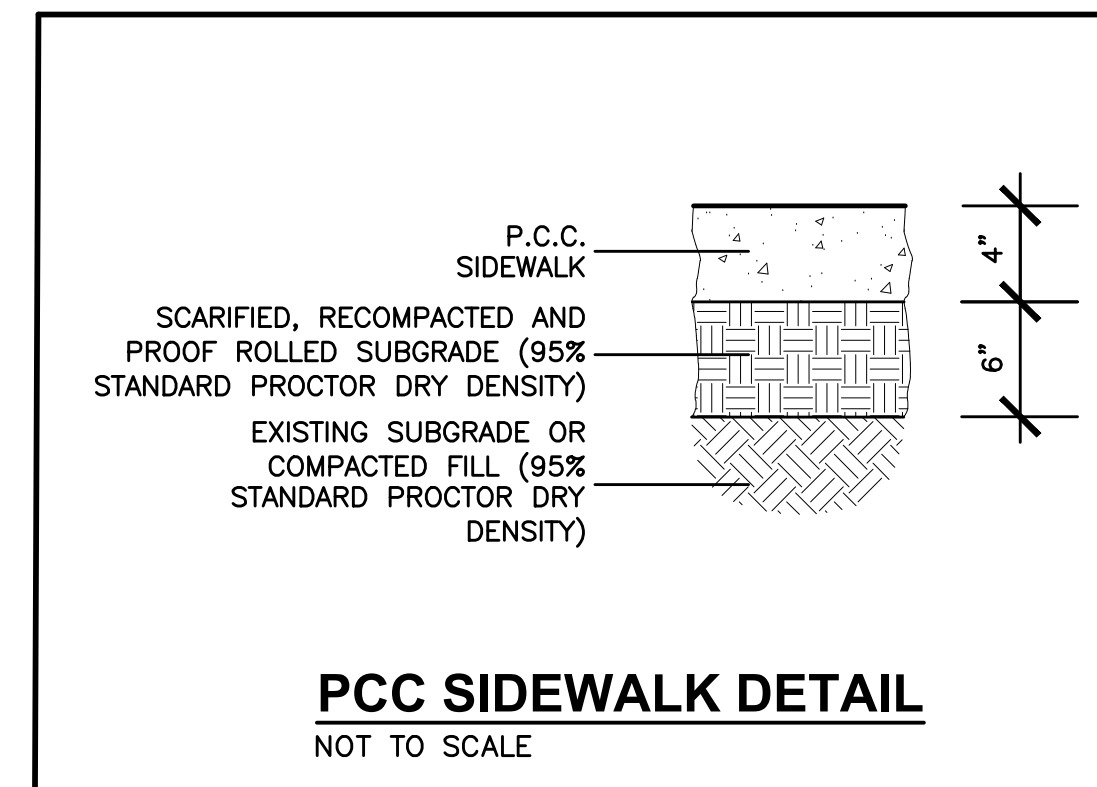
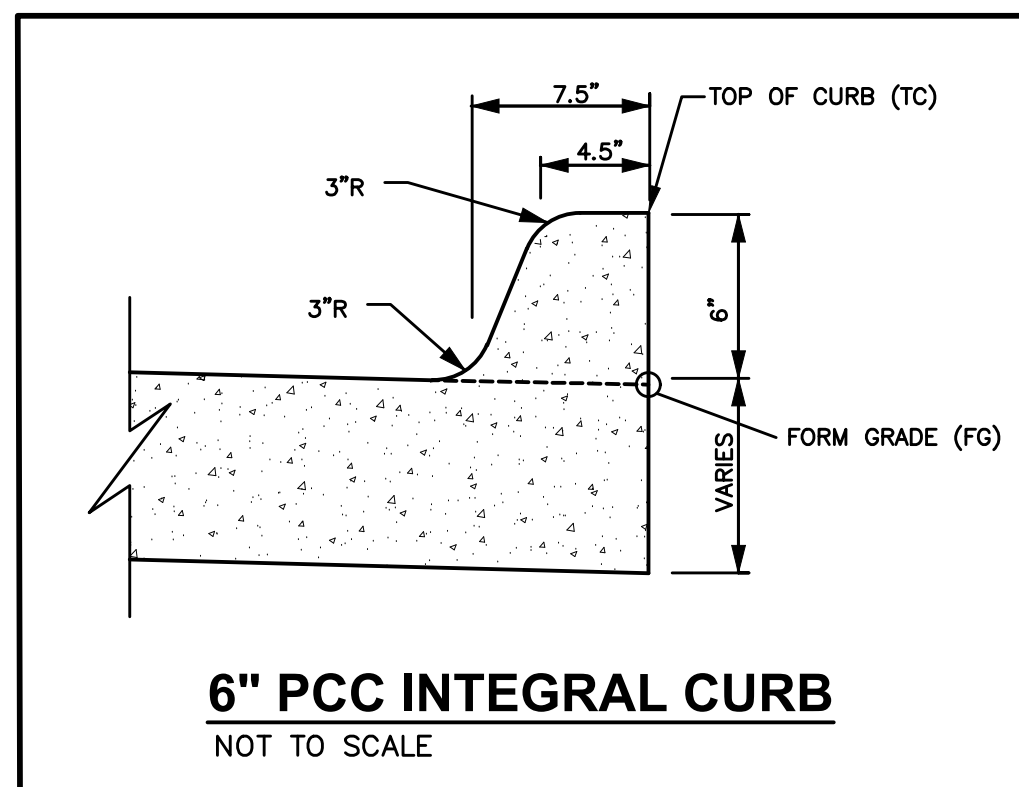
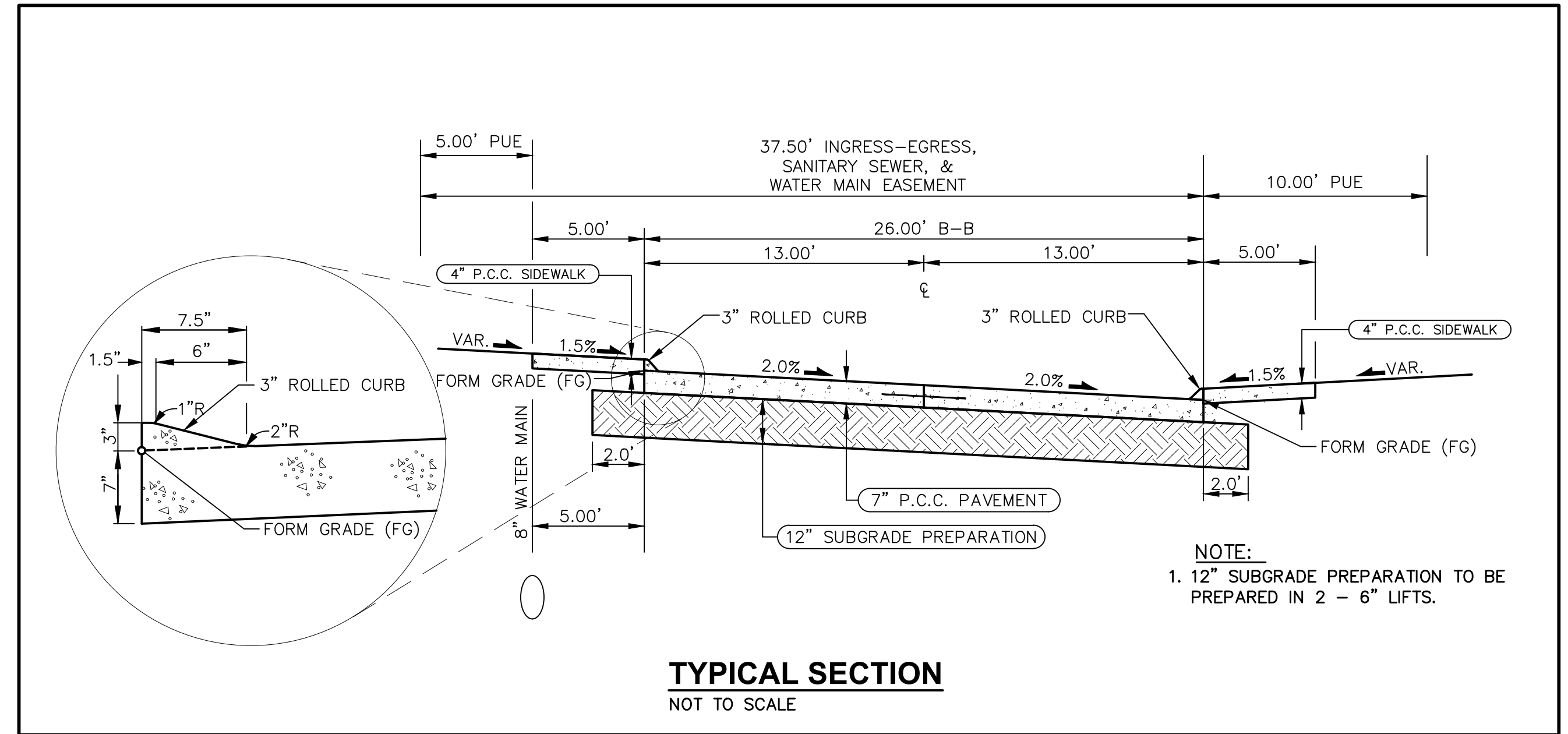
ALL CONSTRUCTION MATERIALS, DUMPSTERS, DETACHED TRAILERS OR SIMILAR ITEMS ARE PROHIBITED ON PUBLIC STREETS OR WITHIN THE PUBLIC R.O.W.

THE 2024 EDITION OF THE SUDAS STANDARD SPECIFICATIONS, THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.

GENERAL NOTES

- WHERE PUBLIC UTILITY FIXTURES ARE SHOWN AS EXISTING ON THE PLANS OR ENCOUNTERED WITHIN THE CONSTRUCTION AREA, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE OWNERS OF THOSE UTILITIES PRIOR TO THE BEGINNING OF ANY CONSTRUCTION. THE CONTRACTOR SHALL AFFORD ACCESS TO THESE FACILITIES FOR NECESSARY MODIFICATION OF SERVICES. UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS, AND THEREFORE, THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. IT IS POSSIBLE THERE MAY BE OTHERS. THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN OR SHOWN. IT IS THE CONTRACTORS RESPONSIBILITY TO DETERMINE THEIR EXISTENCE AND EXACT LOCATIONS AND TO AVOID DAMAGE THERETO. NO CLAIMS FOR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR ANY INTERFERENCE OR DELAY CAUSED BY SUCH WORK.

THE CONTRACTOR IS REQUIRED TO UTILIZE THE UTILITY ONE-CALL SERVICE AT 800-292-8989 AT LEAST 48 HOURS PRIOR TO EXCAVATING ANYWHERE ON THE PROJECT.
- PRIOR TO CONSTRUCTION, CONTRACTOR SHALL NOTIFY (48 HRS NOTICE) THE FOLLOWING:
A. CITY OF ANKENY
B. APPROPRIATE UTILITY COMPANIES
C. OWNER
D. CIVIL DESIGN ADVANTAGE
- THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY AREAS OF PAVEMENT OR SIDEWALK NOT TO BE REMOVED THAT IS DAMAGED DUE TO OPERATING EQUIPMENT ON THE PAVEMENT OR SIDEWALK.
- THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE COORDINATION OF WORK BETWEEN ALL SUPPLIERS AND SUBCONTRACTORS INVOLVED IN THE PROJECT, INCLUDING STAGING OF CONSTRUCTION DETAILS.
- THE CONTRACTOR SHALL APPLY NECESSARY MOISTURE TO THE CONSTRUCTION AREA AND HAUL ROADS TO PREVENT THE SPREAD OF DUST.
- THE CONTRACTOR MAY BE REQUIRED AS DIRECTED BY THE ENGINEER OR THE CITY, TO PLACE TEMPORARY WARNING DEVICES AND SAFETY FENCE AT CERTAIN LOCATIONS WHERE REPLACEMENT FEATURES ARE NOT INSTALLED THE SAME DAY.
- SPECIAL CARE SHALL BE TAKEN WHEN FORMING AT INTERSECTIONS SO THE PROFILES SHOWN ON THE PLANS AND THE ELEVATIONS SHOWN ON THE INTERSECTION DETAILS ARE OBTAINED. SHORT LENGTHS OF FORMS OR FLEXIBLE FORMS MAY BE NECESSARY AT THESE LOCATIONS.
- TO OBTAIN THE CORRECT FORM GRADES AT LOW POINTS WHERE INTAKES ARE LOCATED, THE CONTRACTOR MUST EXERCISE ADDITIONAL CARE WHEN PAVING FULL WIDTH PAVEMENTS. THIS MAY REQUIRE POURING ONE HALF OF THE PAVEMENT AT A TIME OR OTHER METHODS APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL CONFINE HIS GRADING OPERATIONS TO WITHIN THE PROPOSED AND EXISTING RIGHT OF WAY, CONSTRUCTION LIMITS AND EASEMENTS SHOWN ON THE PLANS.
- PLAN AND PROFILE SHEETS INCLUDED IN THE PROJECT ARE FOR THE PURPOSE OF ALIGNMENT, LOCATION AND SPECIFIC DIRECTIONS FOR WORK TO BE PERFORMED UNDER THIS CONTRACT. IRRELEVANT DATA ON THESE SHEETS IS NOT TO BE CONSIDERED A PART OF THIS CONTRACT.
- THE CONTRACTOR IS REQUIRED TO BE FAMILIAR WITH AND COMPLY WITH OSHA STANDARDS. NOTHING WITHIN THESE PLANS SHALL RELIEVE THE CONTRACTOR OF THIS REQUIREMENT.
- CIVIL DESIGN ADVANTAGE WILL PROVIDE ALL CONSTRUCTION STAKING ON THIS PROJECT. NOTIFY MIKE BROONER (369-4400) A MINIMUM OF 48 HOURS BEFORE CONSTRUCTION STAKES ARE REQUIRED.
- THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION AND MAINTENANCE OF ALL EROSION CONTROL MEASURES REQUIRED ON THE STORM WATER POLLUTION PREVENTION PLAN.
- IN THE EVENT OF A DISCREPANCY BETWEEN DETAILED PLANS AND QUANTITIES, THE DETAILED PLANS SHALL GOVERN.
- THE CONTRACTOR IS RESPONSIBLE FOR ALL TRAFFIC CONTROL. ALL TRAFFIC CONTROL SHALL COMPLY WITH MUTCD AND SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION.
- ALL SLOPES IN PAVEMENT SHALL BE UNIFORM TO AVOID PONDING.
- DO NOT STORE CONSTRUCTION MATERIALS AND EQUIPMENT IN THE RIGHT OF WAY.
- ALL PROPERTY PINS SHALL BE PROTECTED FROM GRADING OR OTHER OPERATIONS. ANY PINS DISTURBED SHALL BE RESET AT THE CONTRACTOR'S EXPENSE.
- ALL FIELD TILES ENCOUNTERED SHALL BE REPAIRED AND CONNECTED TO STORM SEWERS WHERE POSSIBLE. LOCATIONS SHALL BE PROVIDED TO ENGINEER FOR NOTATION ON AS-BUILTS INCLUDING LOCATION, TYPE OF TILE, & TILE ELEVATION.
- PRIOR TO ANY WORK AT THIS SITE, CONTRACTOR SHALL EXAMINE ANY APPLICABLE DRAWINGS AVAILABLE FROM THE OWNER OR ENGINEER, AND CONSULT WITH OWNER'S PERSONNEL AND UTILITY COMPANY REPRESENTATIVES. NO COMPENSATION WILL BE ALLOWED FOR DAMAGE FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
- CONTRACTOR SHALL COMPLY WITH ALL P.R.O.W.A.G./A.D.A. REQUIREMENTS FOR ACCESSIBLE SIDEWALK RAMPS INCLUDING RAISED TRUNCATED DOME DETECTABLE WARNINGS.
- CONTRACTOR SHALL NOT DISTURB DESIRABLE GRASS AREAS AND TREES OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR WILL NOT BE PERMITTED TO PARK OR SERVICE VEHICLES AND EQUIPMENT OR USE THESE AREAS FOR STORAGE OF MATERIALS. PARKING AND SERVICE AREAS WILL BE SUBJECT TO THE APPROVAL OF THE OWNER.
- ALL MATERIAL TESTING SHALL BE CONSIDERED INCIDENTAL TO OTHER CONSTRUCTION.
- THE INSTALLATION OF THE STORM SEWER SYSTEM SHALL BE COMPLETED AND FUNCTIONAL PRIOR TO ANY INCREASE IN THE SIZE OF IMPERVIOUS SURFACES WITHIN THE SITE OR THE PREVIOUSLY MENTIONED ITEMS SHALL BE GRADED/INSTALLED AS SOON AS PRACTICAL.
- PROPOSED PUBLIC IMPROVEMENT LAYOUTS SHOWN ARE PRELIMINARY AND SUBJECT TO MODIFICATION WITH THE SUBMITTAL OF PUBLIC IMPROVEMENT CONSTRUCTION DRAWINGS.



FILE: H:\2024\2407540\DWG\2407540-SITE PLAN.DWG
 COMMENT: ENCL
 PLOTTED BY: SAUL HARDING
 DATE: 9/12/2024 3:02 PM

DATE	09/06/2024	THIRD SUBMITTAL
REVISIONS	08/23/2024	SECOND SUBMITTAL
	08/02/2024	FIRST SUBMITTAL

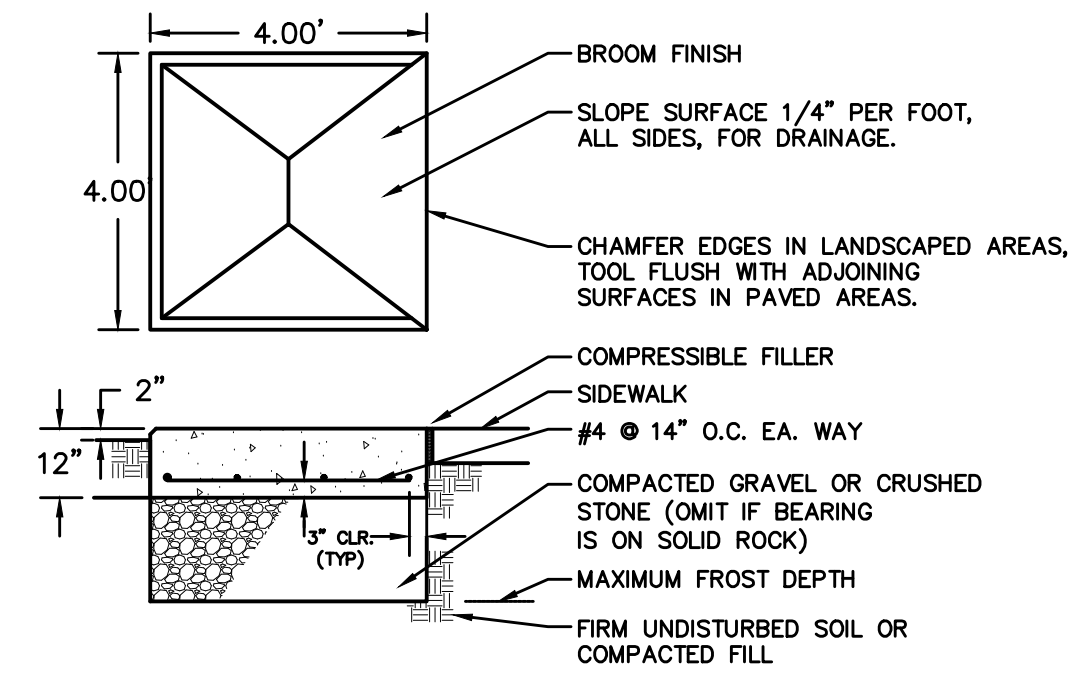
4121 NW URBANDALE DRIVE
URBANDALE, IA 50322
PHONE: (515) 369-4400

TECH: ENGINEER:

DEER CREEK ROWHOMES
DETAILS

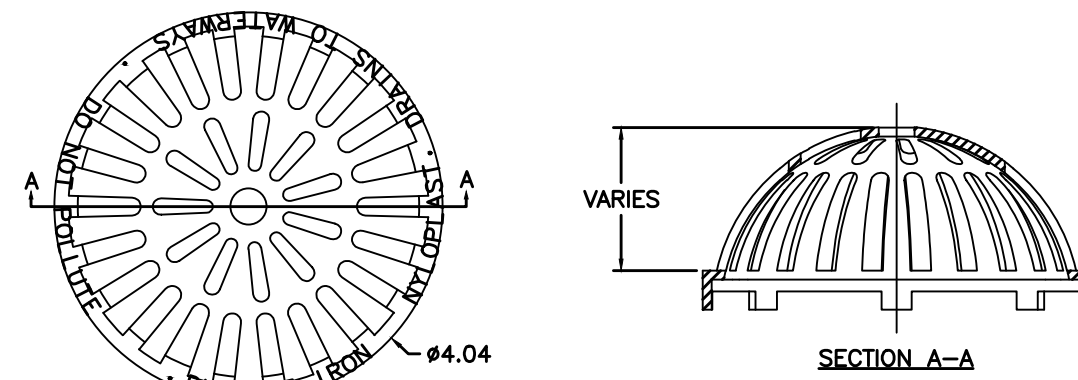
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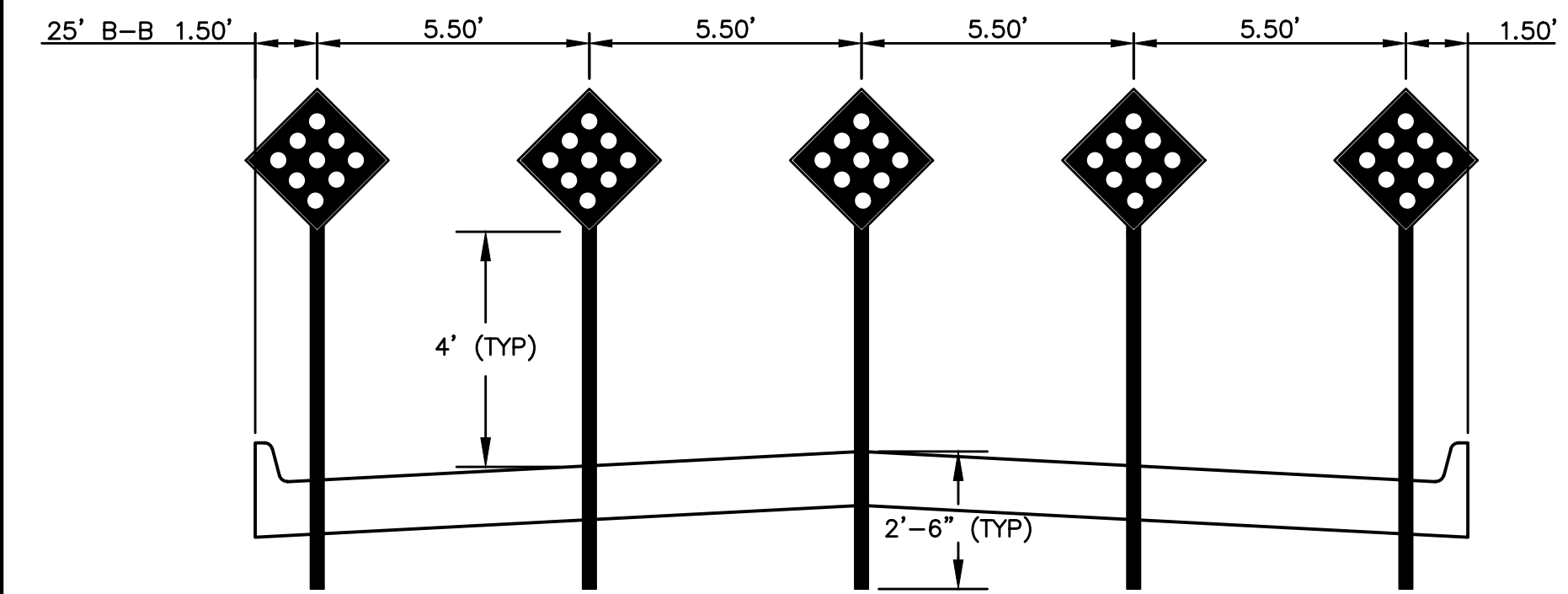
- GENERAL NOTES**
1. CONCRETE: 3000 PSI @ 28 DAYS, 6% AIR ENTRAINMENT, 3" SLUMP, PLACE IN ACCORDANCE WITH ACI-301.
 2. REINFORCING STEEL: ASTM A615, GRADE 60.
 3. THIS BASE DETAIL CAN BE USED FOR NEIGHBORHOOD DELIVERY AND COLLECTION BOX UNITS (NDCBU) CLUSTER BOXES AND PARCEL LOCKERS WITH 4"x10" BOLT PATTERN.
 4. REQUIRED EXPANSION BOLT CAPACITY, BASED ON CALCULATED DESIGN LOAD= 3100 LBS TENSION, 2000 LBS SHEAR.
 5. MAILBOX PADS TO BE INSTALLED AT TIME OF SIDEWALK INSTALLATION.

CLUSTER MAILBOX PAD DETAIL
NOT TO SCALE

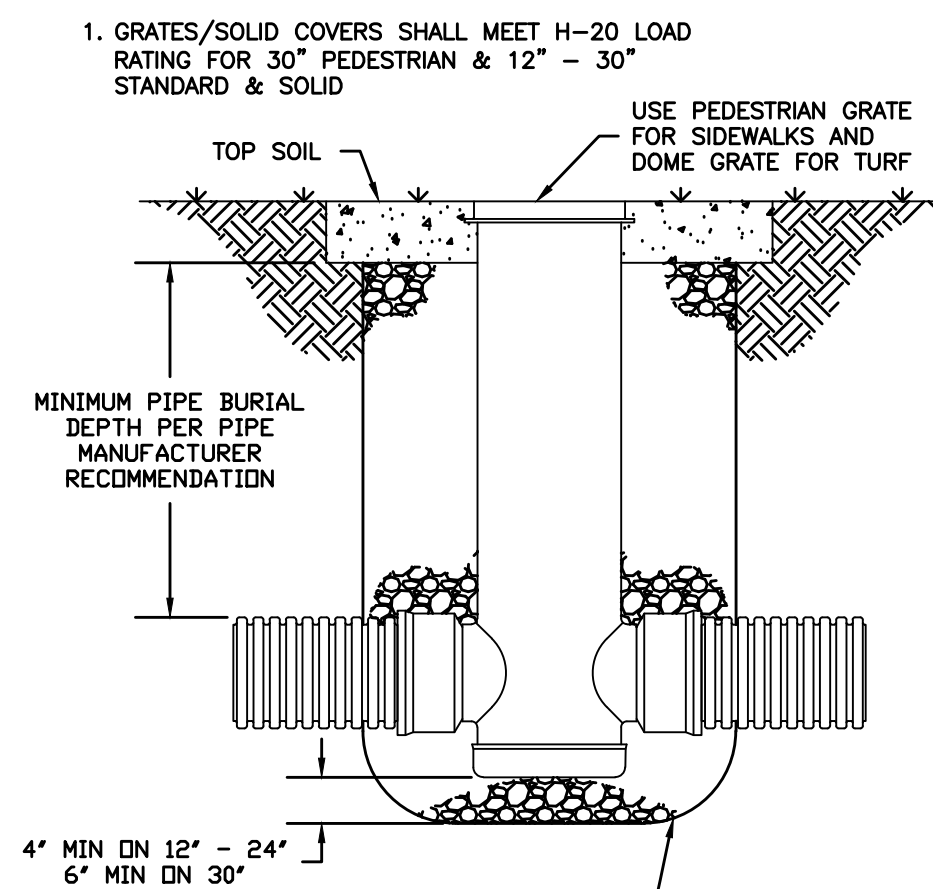


DIMENSIONS ARE FOR REFERENCE ONLY ACTUAL DIMENSIONS MAY VARY
DIMENSIONS ARE IN INCHES
QUALITY: MATERIALS SHALL CONFORM TO ASTM A536 GRADE 70-50-05
PAINT: CASTINGS ARE FURNISHED WITH A BLACK PAINT LOCKING DEVICE AVAILABLE UPON REQUEST

DOME GRATE
NOT TO SCALE

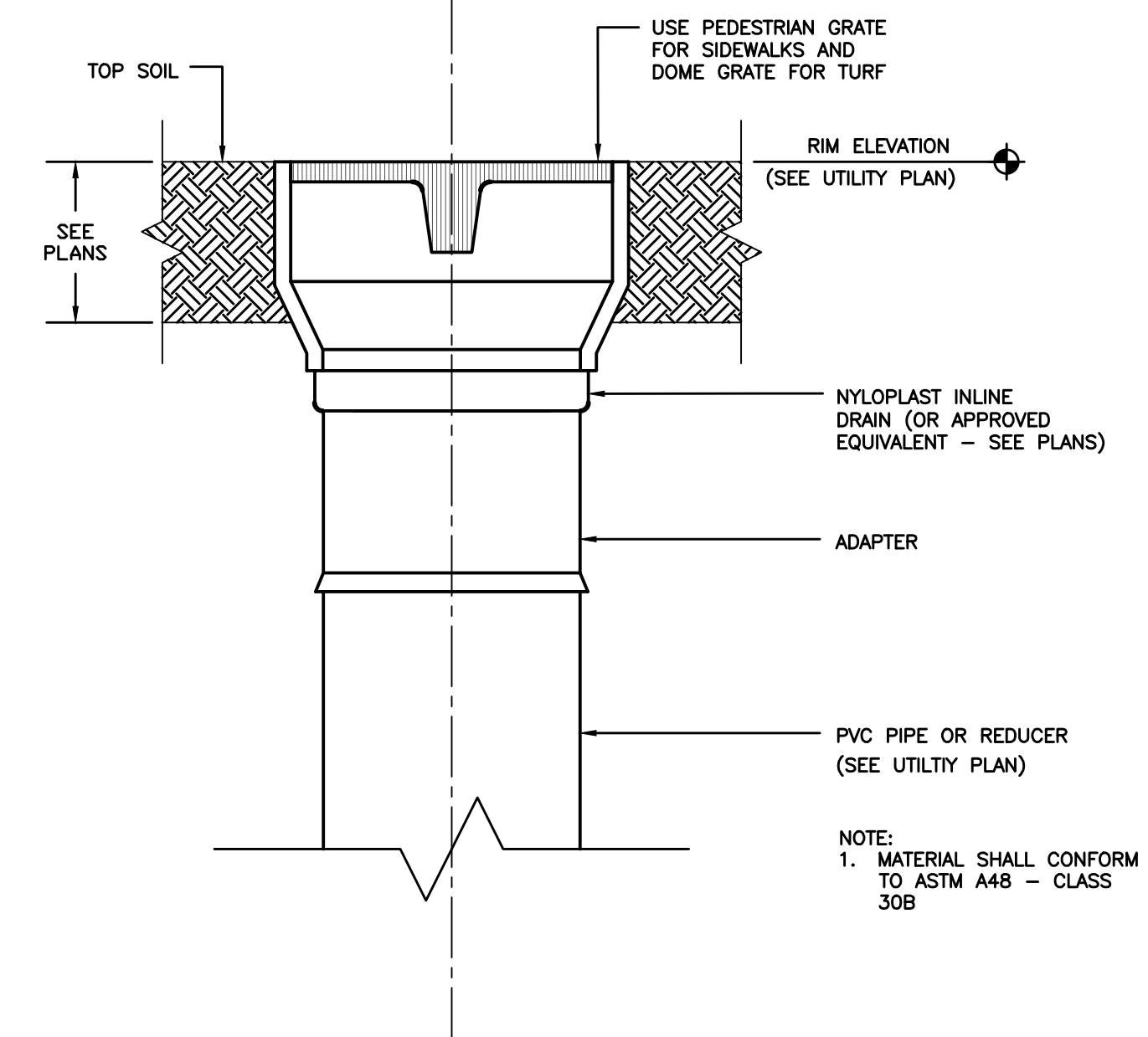


PERMANENT ROAD CLOSURE SIGN SPACING DETAIL
(IOWA DOT SI-182)
NOT TO SCALE



THE BACKFILL MATERIAL SHALL BE CRUSHED STONE OR OTHER GRANULAR MATERIAL MEETING THE REQUIREMENTS OF CLASS II MATERIAL AS DEFINED IN ASTM D2321, OR AS DETERMINED BY LOCAL STANDARDS & SITE ENGINEER. BEDDING & BACKFILL FOR SURFACE DRAINAGE INLETS SHALL BE PLACED & COMPACTED UNIFORMLY IN ACCORDANCE WITH ASTM D2321.

HDPE INTAKE
NOT TO SCALE



NOTE:
1. MATERIAL SHALL CONFORM TO ASTM A48 - CLASS 30B

HDPE INLINE DRAIN
NOT TO SCALE

FILE: H:\2024\2407540\DWG\2407540-SITE PLAN.DWG
COMMENT: DWG
PLOTTED BY: PAUL HARDING
DATE: 9/17/2024 3:02 PM

DATE	REVISIONS
08/06/2024	THIRD SUBMITTAL
08/23/2024	SECOND SUBMITTAL
08/02/2024	FIRST SUBMITTAL

4121 NW URBANDALE DRIVE
URBANDALE, IA 50322
PHONE: (515) 369-4400

TECH:
ENGINEER:

ANKENY, IOWA

DEER CREEK ROWHOMES
DETAILS



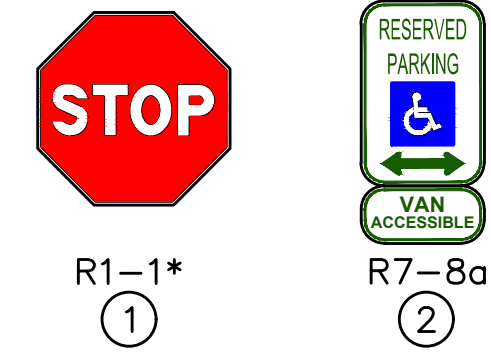
GENERAL NOTES

1. THE 2024 EDITION OF THE SUDAS STANDARD SPECIFICATIONS AND ALL CITY OF ANKENY SUPPLEMENTAL SPECIFICATIONS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.
2. ALL WORK SHALL COMPLY WITH ADA ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES.
3. ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.
4. PRIOR TO ANY WORK AT THE SITE, CONTRACTOR SHALL EXAMINE ANY APPLICABLE DRAWINGS AVAILABLE FROM THE OWNER, ENGINEER, AND/OR ARCHITECT, AND CONSULT WITH OWNER'S PERSONNEL AND UTILITY COMPANY REPRESENTATIVES. NO COMPENSATION WILL BE ALLOWED FOR DAMAGE FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
5. ONE WEEK PRIOR TO CONSTRUCTION WITHIN CITY R.O.W. OR ANY CONNECTION TO PUBLIC SEWERS CONTRACTOR SHALL NOTIFY THE CITY OF ANKENY DEVELOPMENT ENGINEERING.
6. ALL CONSTRUCTION WITHIN PUBLIC R.O.W./ EASEMENTS, AND/OR ANY CONNECTION TO PUBLIC SEWERS AND STREETS, SHALL COMPLY WITH THE CITY OF ANKENY SUPPLEMENTAL SPECIFICATIONS.
7. ALL DIMENSIONS ARE TO BACK OF CURB, BUILDING FACE OR PROPERTY LINE UNLESS OTHERWISE NOTED.
8. CONTRACTOR TO VERIFY BUILDING DIMENSIONS WITH ARCHITECTURAL PLANS.
9. PLACE 3/4" INCH EXPANSION JOINT BETWEEN ALL P.C.C. PAVEMENT/SIDEWALKS AND BUILDING. PLACE 1/2" INCH EXPANSION JOINT BETWEEN SIDEWALKS AND P.C.C. PAVEMENT.
10. REMOVE ALL DEBRIS SPILLED INTO R.O.W. AT THE END OF EACH WORK DAY.
11. ALL PROPERTY PINS SHALL BE PROTECTED FROM GRADING OR OTHER OPERATIONS. ANY PINS DISTURBED SHALL BE RESET AT THE CONTRACTOR'S EXPENSE.
12. DO NOT STORE CONSTRUCTION MATERIALS AND EQUIPMENT IN THE RIGHT OF WAY.
13. THE CONTRACTOR SHALL NOT DISTURB DESIRABLE GRASS AREAS AND DESIRABLE TREES OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR WILL NOT BE PERMITTED TO PARK OR SERVICE VEHICLES AND EQUIPMENT OR USE THESE AREAS FOR STORAGE OF MATERIALS, STORAGE, PARKING AND SERVICE AREAS WILL BE SUBJECT TO THE APPROVAL OF THE OWNER.
14. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY AREAS OF PAVEMENT OR SIDEWALK NOT TO BE REMOVED THAT IS DAMAGED DUE TO OPERATING EQUIPMENT ON THE PAVEMENT OR SIDEWALK.
15. THE CONTRACTOR MAY BE REQUIRED TO PLACE TEMPORARY WARNING DEVICES AND SAFETY FENCE AT CERTAIN LOCATIONS WHERE REPLACEMENT FEATURES ARE NOT INSTALLED THE SAME DAY, AS DIRECTED BY THE ENGINEER OR THE CITY.
16. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE COORDINATION OF WORK BETWEEN SUPPLIERS AND SUBCONTRACTORS INVOLVED IN THE PROJECT, INCLUDING STAGING OF CONSTRUCTION DETAILS.
17. CONCRETE REMOVAL FOR DRIVEWAY APPROACHES SHALL BE REMOVED TO THE NEAREST TRANSVERSE JOINT. CONTRACTOR SHALL VERIFY REMOVAL LIMITS WITH CITY'S CONSTRUCTION INSPECTOR PRIOR TO ANY CONCRETE REMOVAL.
18. ALL PERMITS AND ADDITIONAL FEES REQUIRED TO COMPLETE THE WORK SHALL BE INCLUDED IN THE CONTRACTOR BID.
19. ANY DEVIATIONS IN DESIGN FROM THE APPROVED SITE PLAN SHALL REQUIRE REVIEW AND APPROVAL FROM THE CITY OF ANKENY.
20. STOP SIGNS AT INTERSECTIONS WITH PUBLIC STREETS SHALL BE INSTALLED PRIOR TO ANY T.C.O. OR C.O. BEING ISSUED.

TRAFFIC CONTROL NOTES

1. ALL APPLICABLE CITY PERMITS, INCLUDING BUT NOT LIMITED TO CLOSURE PERMITS, SHALL BE OBTAINED PRIOR TO ANY CONSTRUCTION WITHIN CITY R.O.W. OR LANE CLOSURES.
2. ALL TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
3. PERMANENT SIGNING THAT CONVEYS A MESSAGE CONTRARY TO THE MESSAGE OF TEMPORARY SIGNING AND NOT APPLICABLE TO THE WORKING CONDITIONS SHALL BE COVERED BY THE CONTRACTOR WHEN DIRECTED BY THE CITY.
4. THE CONTRACTOR SHALL COORDINATE HIS TRAFFIC CONTROL WITH OTHER CONSTRUCTION PROJECTS IN THE AREA.
5. SIDEWALK CLOSED SIGNS REQUIRED FOR ALL SIDEWALK CLOSURES.
6. THE CONTRACTOR IS CAUTIONED NEITHER TO OBSTRUCT NOR REMOVE ANY EXISTING PAVEMENT, NOR TO DISTURB THE EXISTING TRAFFIC PATTERNS MORE THAN IS NECESSARY FOR THE PROPER EXECUTION OF THE WORK.
7. ALL SIGNING AND LANE STRIPING WILL NEED TO COMPLY WITH MUTCD. MAINTENANCE AND REPLACEMENT OF THE SIGNING AND STRIPING WILL BE THE RESPONSIBILITY OF THE APPLICANT.
8. THE DEVELOPER IS RESPONSIBLE FOR STOP SIGN R1-1 (30") INSTALLATION AND MAINTENANCE FOR STOPPING PRIVATE STREET TRAFFIC AT INTERSECTIONS WITH PUBLIC STREETS. ANY STREET NAME SIGNS IF DESIRED SHALL ALSO BE THE RESPONSIBILITY OF THE DEVELOPER.
9. THE DEVELOPER IS RESPONSIBLE FOR "ROAD ENDS" SIGNAGE INSTALLATION AND MAINTENANCE.

SIGN LEGEND



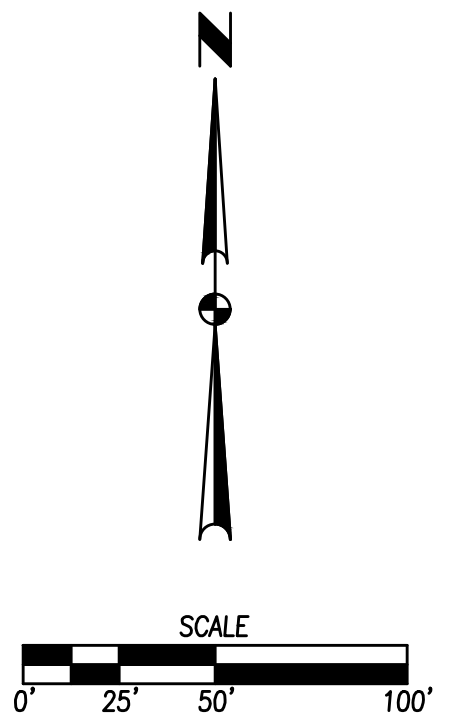
*STOP SIGNS ARE TO BE 36" AT INTERSECTIONS WITH PUBLIC STREETS AND 30" AT PRIVATE STREET INTERSECTIONS.

PAVEMENT THICKNESS

- | | |
|--------------------------|-----------|
| 1. SIDEWALKS | 4" P.C.C. |
| 2. SIDEWALK RAMPS | 6" P.C.C. |
| 3. DRIVE AISLES | 7" P.C.C. |
| 4. PARKING SPACES | 6" P.C.C. |
| 5. SIDEWALK AT DRIVEWAYS | 6" P.C.C. |
| 6. DRIVEWAYS | 6" P.C.C. |

PROJECT PHASING

- | | |
|----------|-------------|
| PHASE 1: | LOTS 1-60 |
| FUTURE: | LOTS 61-124 |



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 DATE: 5/8/2024 3:02 PM

DEER CREEK ROWHOMES
DIMENSION PLAN

3.0

2407.540

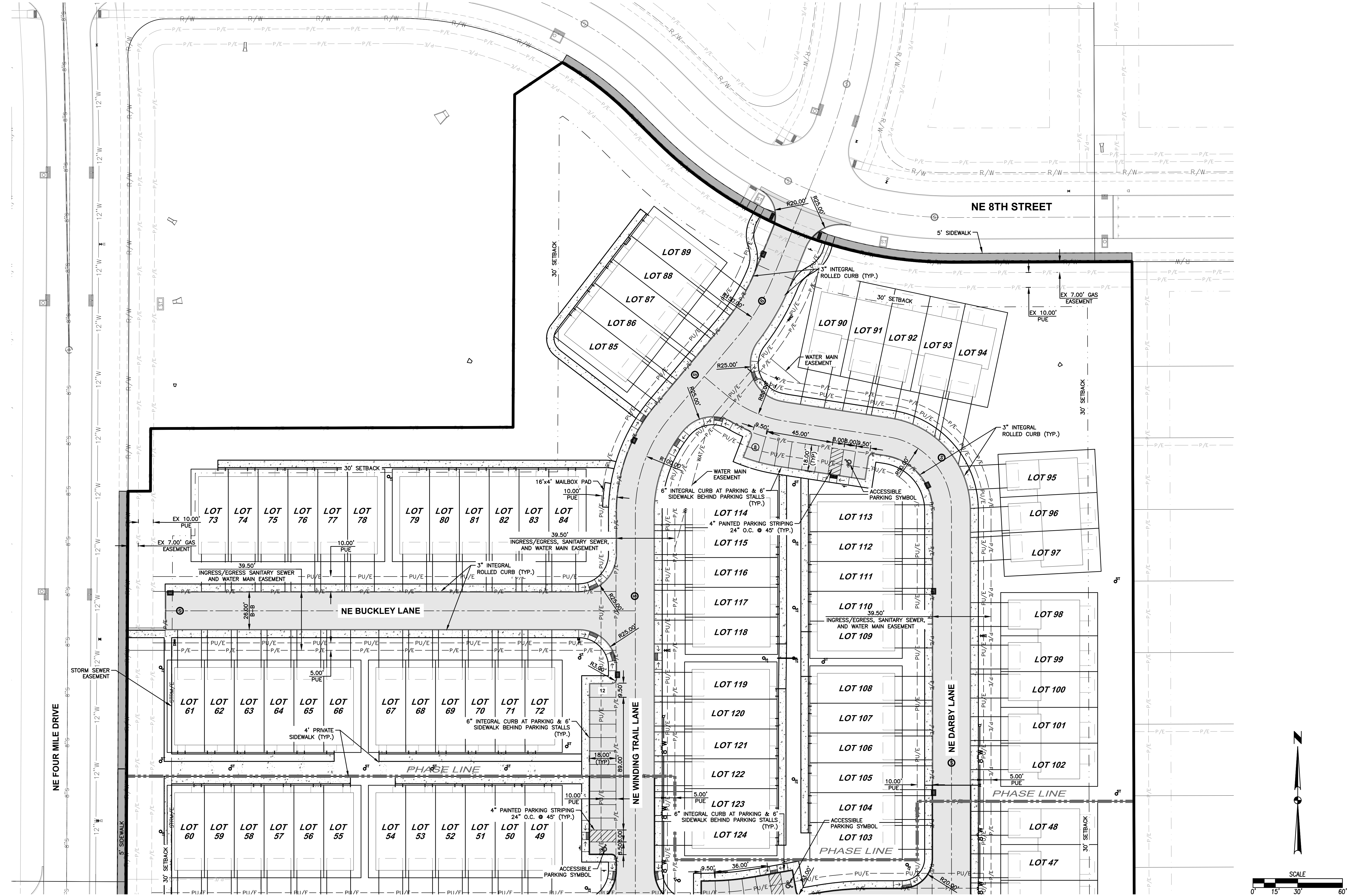
CIVIL DESIGN ADVANTAGE ENGINEER:
ANKENY, IOWA

4121 NW URBANDALE DRIVE
URBANDALE, IA 50322
PHONE: (515) 369-4400

DATE	REVISIONS
08/06/2024	THIRD SUBMITTAL
08/23/2024	SECOND SUBMITTAL
08/02/2024	FIRST SUBMITTAL

TECH: _____

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DATE	REVISIONS
08/06/2024	THIRD SUBMITTAL
08/23/2024	SECOND SUBMITTAL
08/02/2024	FIRST SUBMITTAL

4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 369-4400



ANKENY, IOWA

DEER CREEK ROWHOMES

DIMENSION PLAN

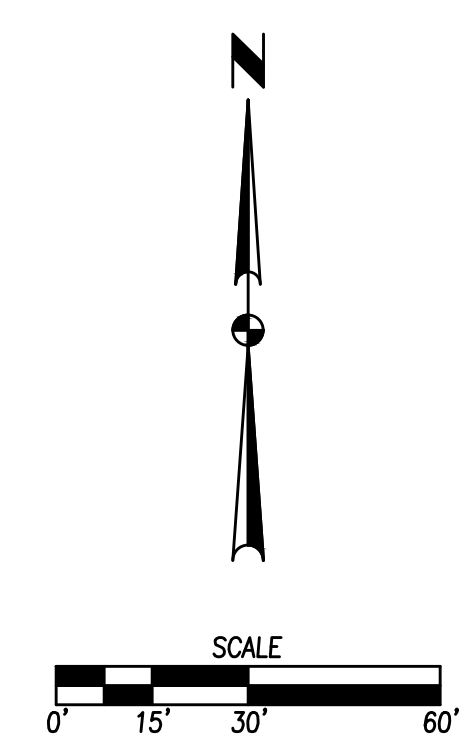
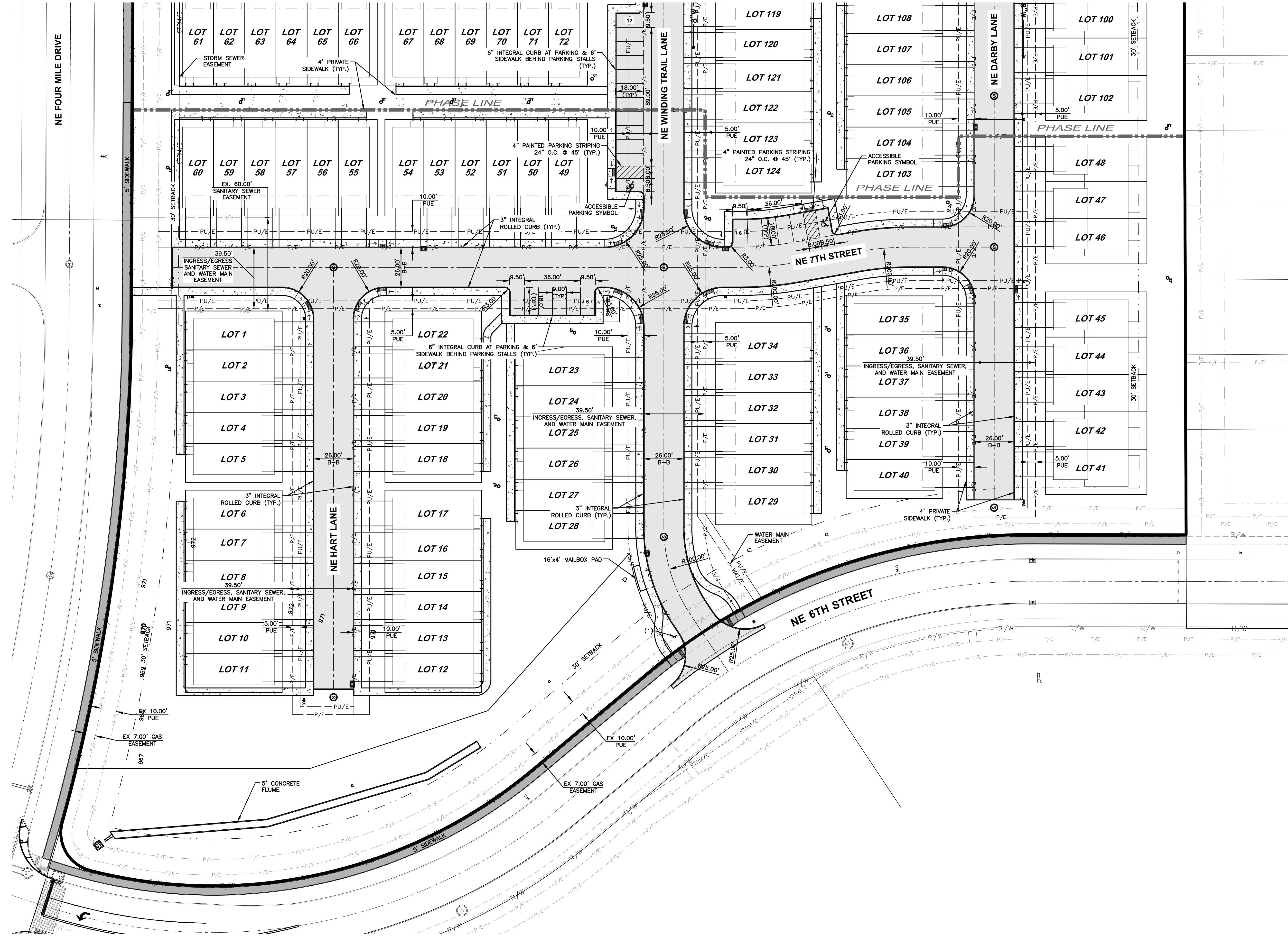
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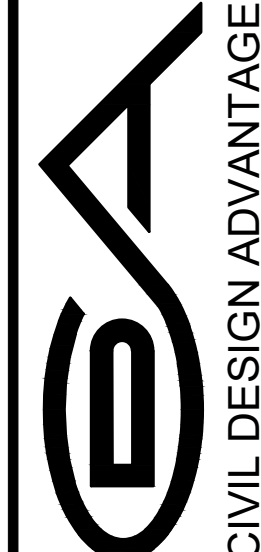
DEER CREEK ROWHOMES

DIMENSION PLAN

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ANKENY, IOWA

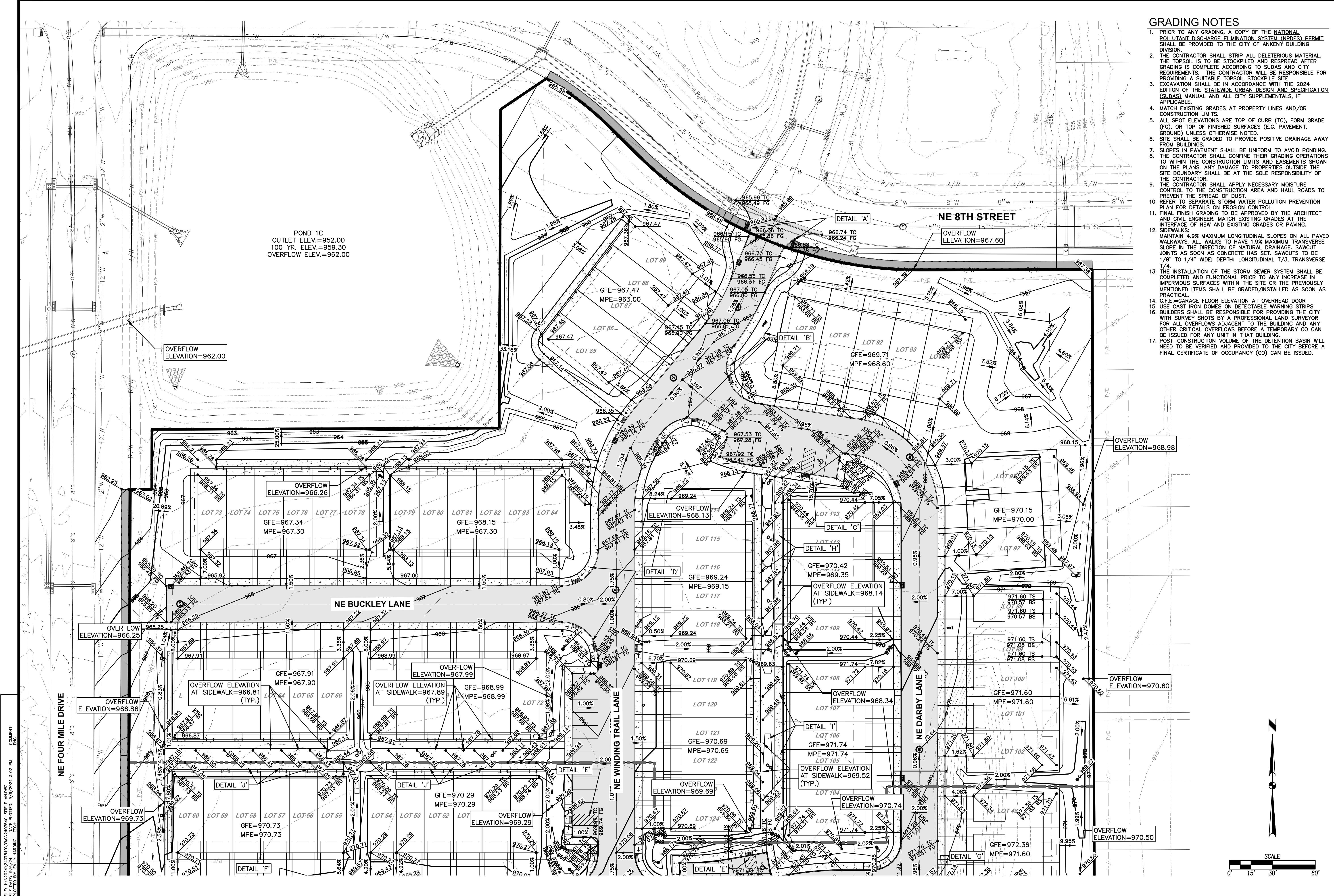


CIVIL DESIGN ADVANTAGE ENGINEER:

4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 369-4400

TECH:

DATE	REVISIONS
08/06/2024	THIRD SUBMITTAL
08/23/2024	SECOND SUBMITTAL
08/02/2024	FIRST SUBMITTAL



POND 1C
 OUTLET ELEV.=952.00
 100 YR. ELEV.=959.30
 OVERFLOW ELEV.=962.00

OVERFLOW
 ELEVATION=962.00

OVERFLOW
 ELEVATION=966.26

OVERFLOW
 ELEVATION=968.13

OVERFLOW
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OVERFLOW
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OVERFLOW
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OVERFLOW
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OVERFLOW
 ELEVATION=967.99

OVERFLOW ELEVATION
 AT SIDEWALK=967.89
 (TYP.)

OVERFLOW
 ELEVATION=969.69

OVERFLOW ELEVATION
 AT SIDEWALK=969.52
 (TYP.)

OVERFLOW
 ELEVATION=970.74

OVERFLOW
 ELEVATION=970.60

OVERFLOW
 ELEVATION=970.50

GRADING NOTES

1. PRIOR TO ANY GRADING, A COPY OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT SHALL BE PROVIDED TO THE CITY OF ANKENY BUILDING DIVISION.
2. THE CONTRACTOR SHALL STRIP ALL DELETERIOUS MATERIAL. THE TOPSOIL IS TO BE STOCKPILED AND RESPAVED AFTER GRADING IS COMPLETE ACCORDING TO SUDAS AND CITY REQUIREMENTS. THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING A SUITABLE TOPSOIL STOCKPILE SITE.
3. EXCAVATION SHALL BE IN ACCORDANCE WITH THE 2024 EDITION OF THE STATEWIDE URBAN DESIGN AND SPECIFICATION (SUDAS) MANUAL AND ALL CITY SUPPLEMENTALS, IF APPLICABLE.
4. MATCH EXISTING GRADES AT PROPERTY LINES AND/OR CONSTRUCTION LIMITS.
5. ALL SPOT ELEVATIONS ARE TOP OF CURB (TC), FORM GRADE (FG), OR TOP OF FINISHED SURFACES (E.G. PAVEMENT, GROUND) UNLESS OTHERWISE NOTED.
6. SITE SHALL BE GRADED TO PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDINGS.
7. SLOPES IN PAVEMENT SHALL BE UNIFORM TO AVOID PONDING.
8. THE CONTRACTOR SHALL CONFINE THEIR GRADING OPERATIONS TO WITHIN THE CONSTRUCTION LIMITS AND EASEMENTS SHOWN ON THE PLANS. ANY DAMAGE TO PROPERTIES OUTSIDE THE SITE BOUNDARY SHALL BE AT THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL APPLY NECESSARY MOISTURE CONTROL TO THE CONSTRUCTION AREA AND HAUL ROADS TO PREVENT THE SPREAD OF DUST.
10. REFER TO SEPARATE STORM WATER POLLUTION PREVENTION PLAN FOR DETAILS ON EROSION CONTROL.
11. FINAL FINISH GRADING TO BE APPROVED BY THE ARCHITECT AND CIVIL ENGINEER. MATCH EXISTING GRADES AT THE INTERFACE OF NEW AND EXISTING GRADES OR PAVING.
12. SIDEWALKS:
 MAINTAIN 4.9% MAXIMUM LONGITUDINAL SLOPES ON ALL PAVED WALKWAYS. ALL WALKS TO HAVE 1.9% MAXIMUM TRANSVERSE SLOPE IN THE DIRECTION OF NATURAL DRAINAGE. SAWCUT JOINTS AS SOON AS CONCRETE HAS SET. SAWCUTS TO BE 1/8" TO 1/4" WIDE; DEPTH: LONGITUDINAL 1/3, TRANSVERSE 1/4.
13. THE INSTALLATION OF THE STORM SEWER SYSTEM SHALL BE COMPLETED AND FUNCTIONAL PRIOR TO ANY INCREASE IN IMPERVIOUS SURFACES WITHIN THE SITE OR THE PREVIOUSLY MENTIONED ITEMS SHALL BE GRADED/INSTALLED AS SOON AS PRACTICAL.
14. G.F.E.=GARAGE FLOOR ELEVATION AT OVERHEAD DOOR
15. USE CAST IRON DOMES ON DETECTABLE WARNING STRIPS.
16. BUILDERS SHALL BE RESPONSIBLE FOR PROVIDING THE CITY WITH SURVEY SHOTS BY A PROFESSIONAL LAND SURVEYOR FOR ALL OVERFLOWS ADJACENT TO THE BUILDING AND ANY OTHER CRITICAL OVERFLOWS BEFORE A TEMPORARY CO CAN BE ISSUED FOR ANY UNIT IN THAT BUILDING.
17. POST-CONSTRUCTION VOLUME OF THE DETENTION BASIN WILL NEED TO BE VERIFIED AND PROVIDED TO THE CITY BEFORE A FINAL CERTIFICATE OF OCCUPANCY (CO) CAN BE ISSUED.

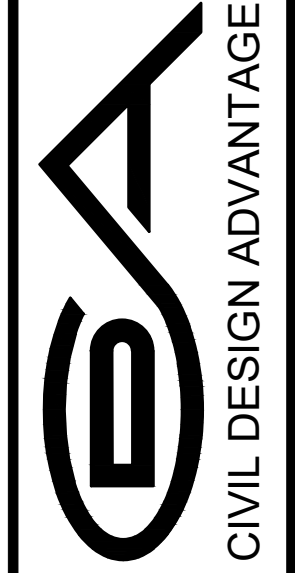
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DATE	REVISIONS
08/06/2024	
08/23/2024	
08/02/2024	

THIRD SUBMITTAL	SECOND SUBMITTAL	FIRST SUBMITTAL

4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 369-4400

TECH:
 ENGINEER:



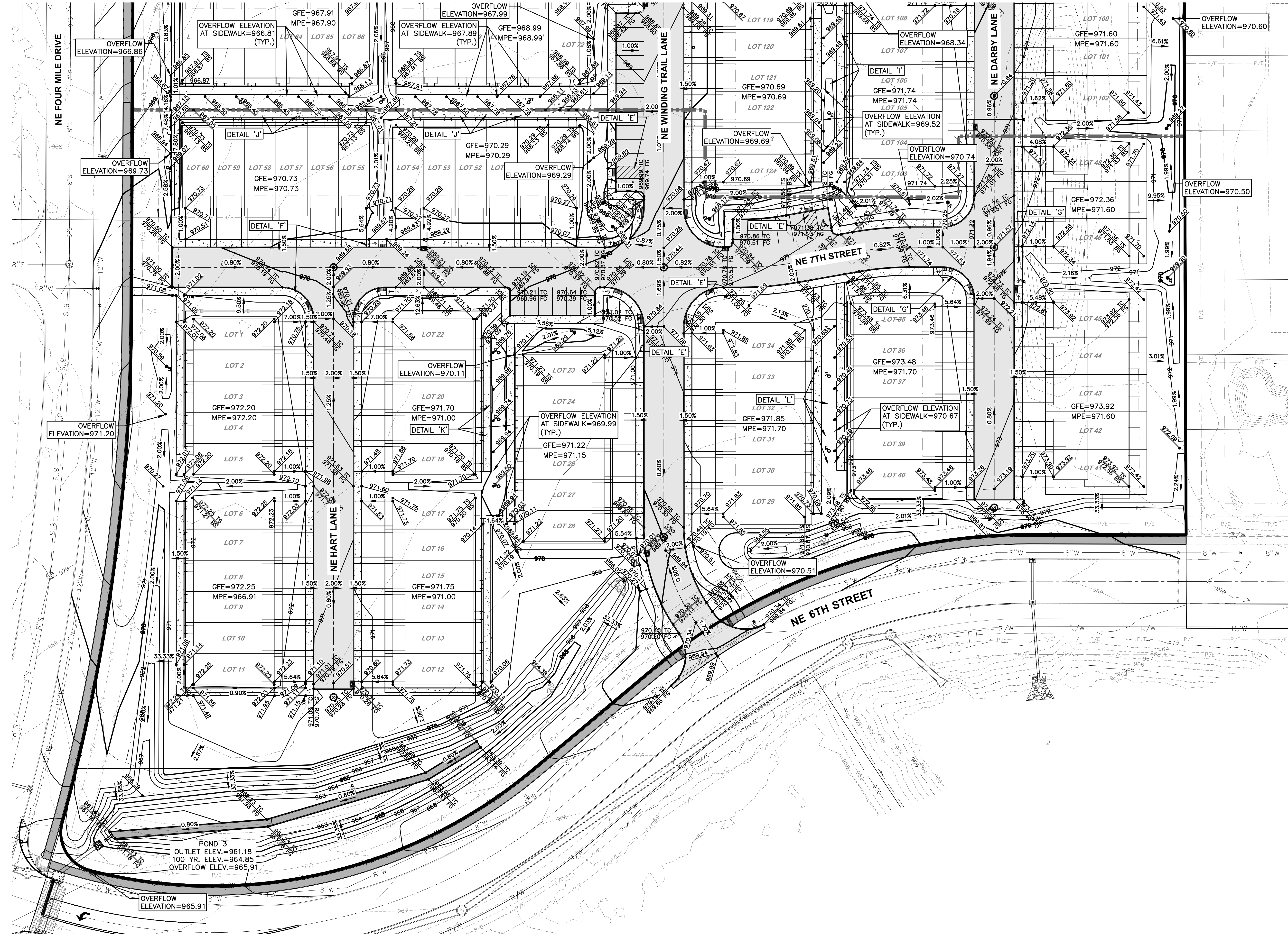
CIVIL DESIGN ADVANTAGE
 ANKENY, IOWA

DEER CREEK ROWHOMES
GRADING PLAN

4.0

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DATE	REVISIONS
08/06/2024	THIRD SUBMITTAL
08/23/2024	SECOND SUBMITTAL
08/02/2024	FIRST SUBMITTAL

4121 NW URBANDALE DRIVE
URBANDALE, IA 50322
PHONE: (515) 369-4400

ESA
CIVIL DESIGN ADVANTAGE

ANKENY, IOWA

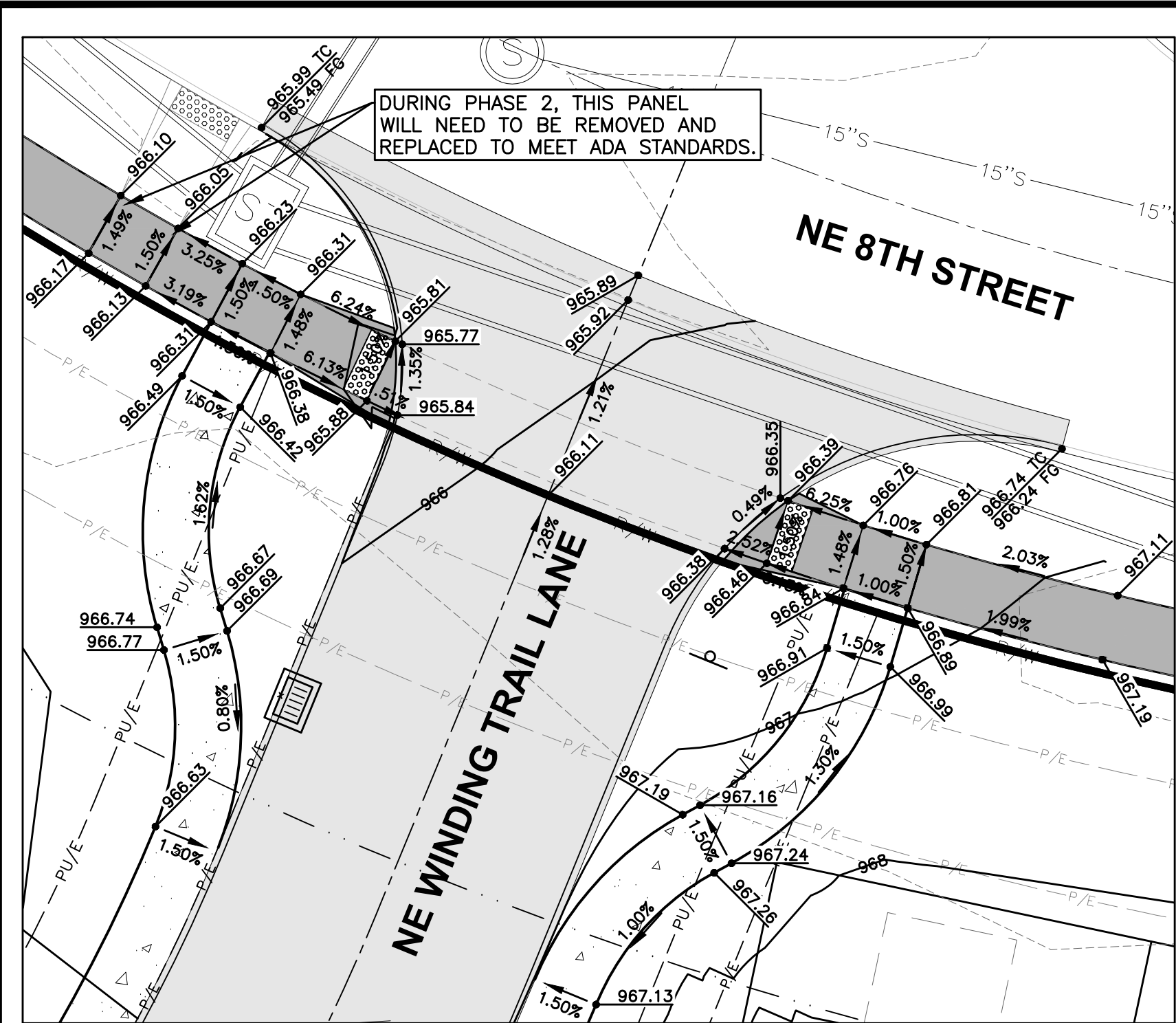
DEER CREEK ROWHOMES GRADING PLAN

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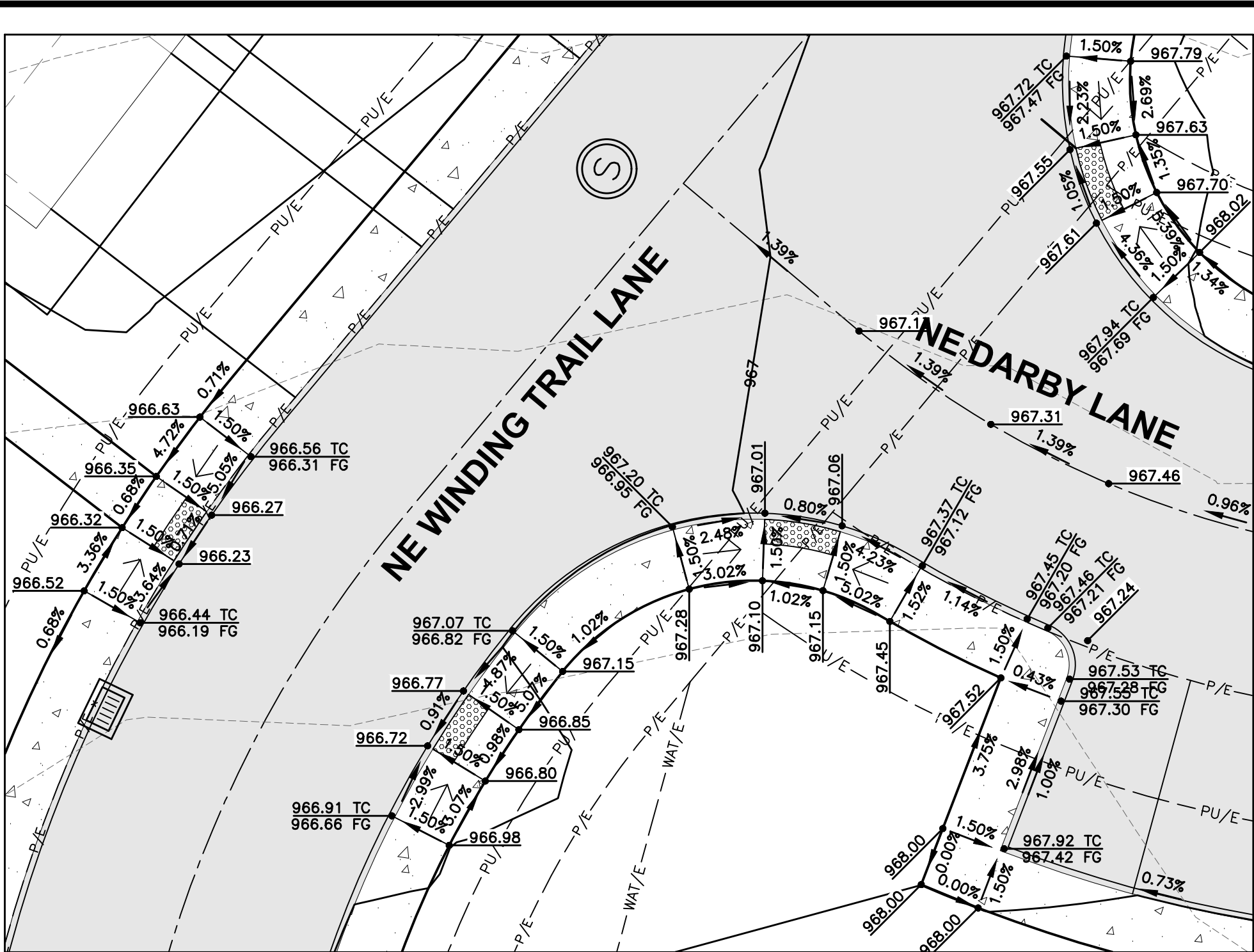
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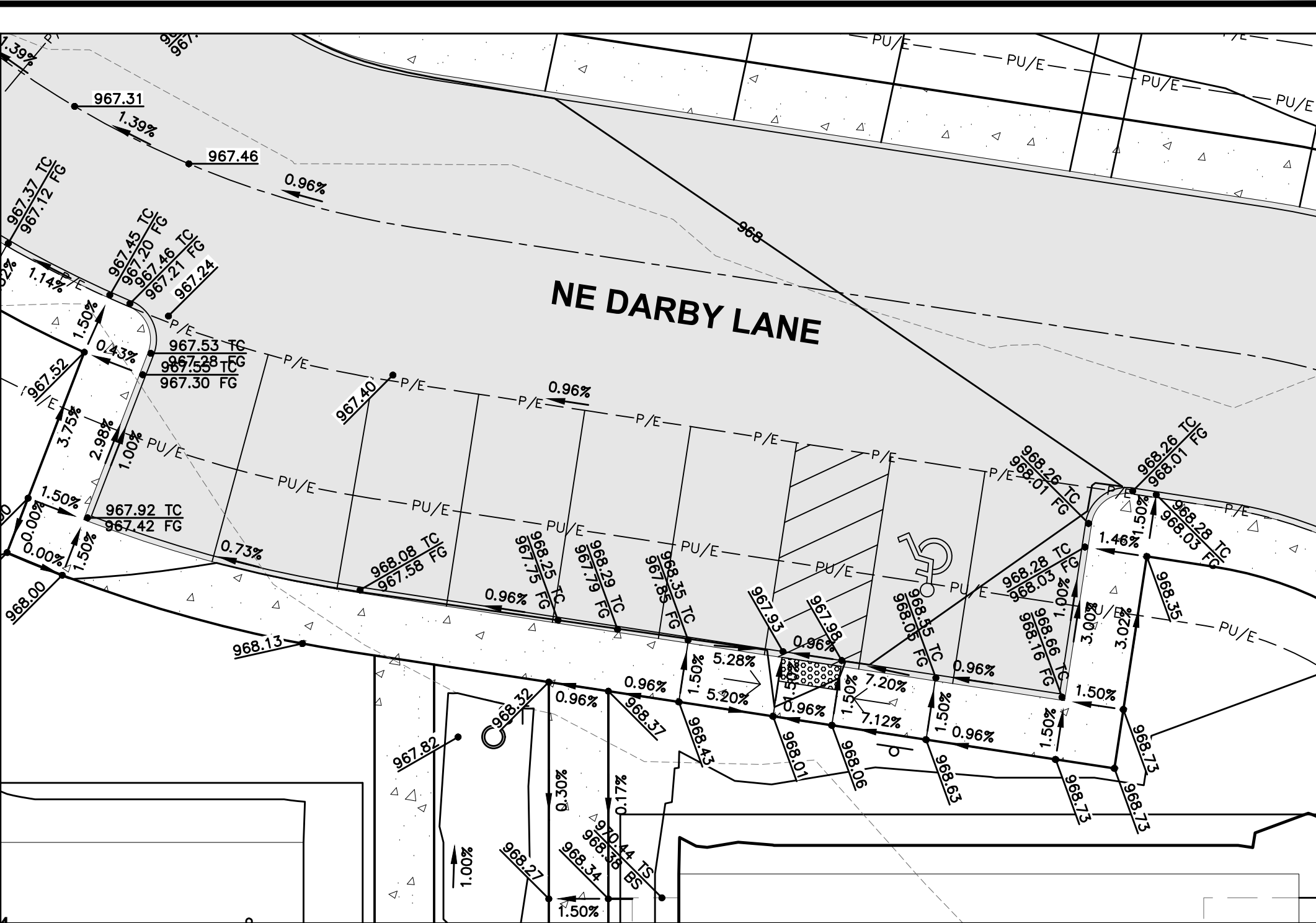
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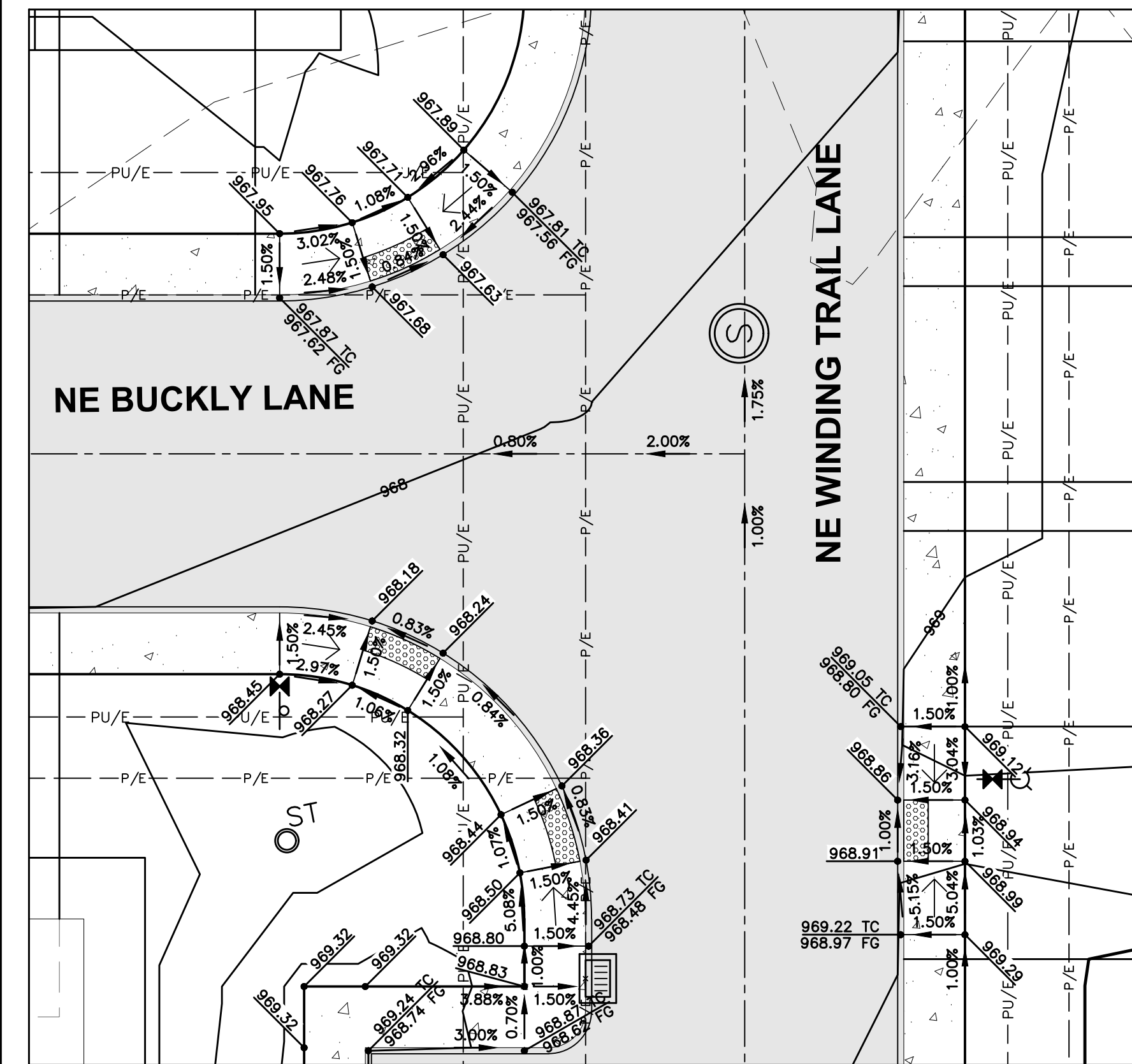
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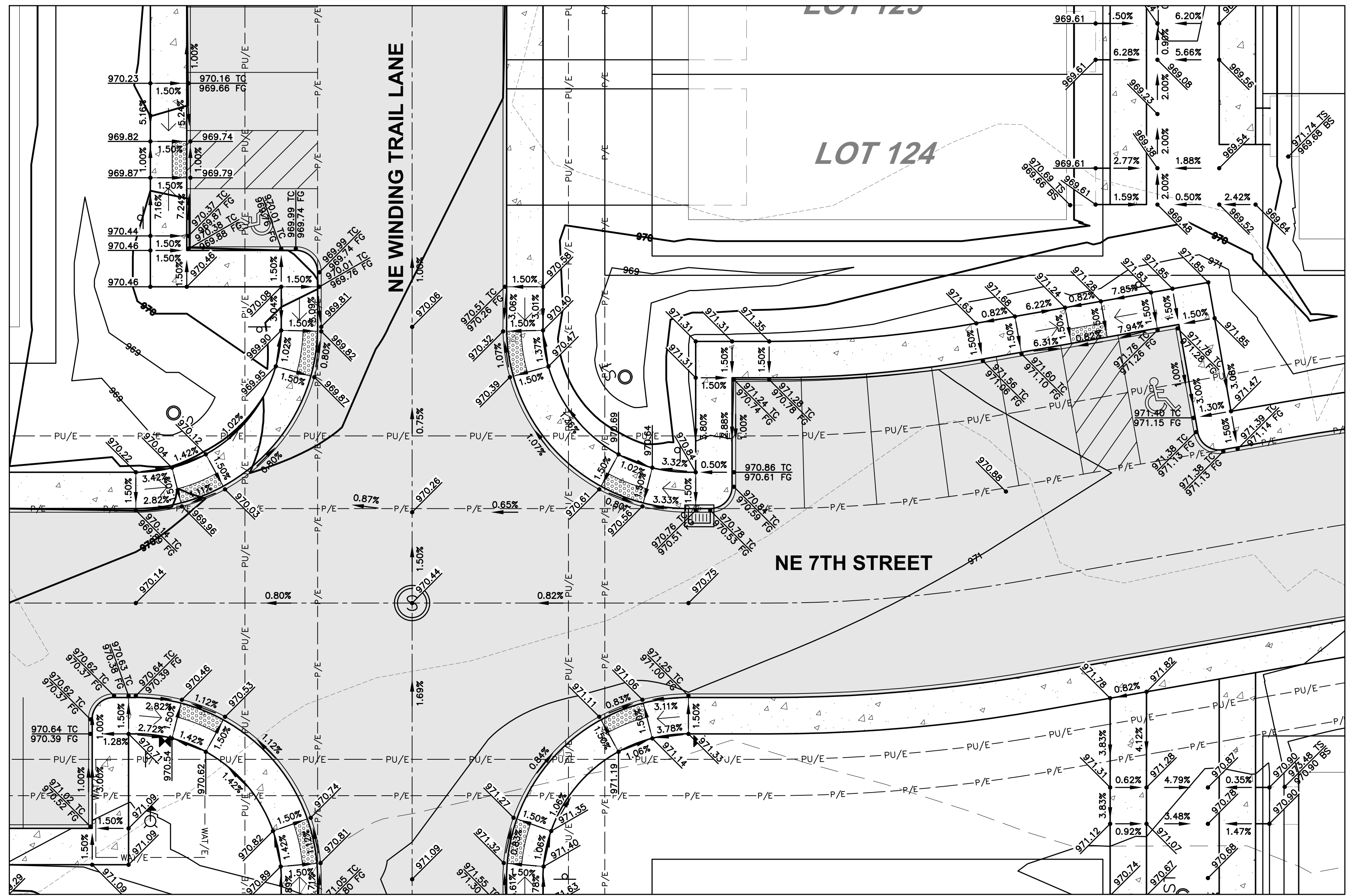
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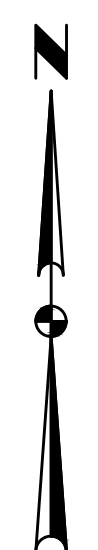
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DETAIL 'D' (1"=10' FULL-SIZE)



DETAIL 'E' (1"=10' FULL-SIZE)



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DATE	REVISIONS
08/02/2024	THIRD SUBMITTAL
08/23/2024	SECOND SUBMITTAL
08/02/2024	FIRST SUBMITTAL

4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 369-4400
 TECH:

ENGINEER:
 CIVIL DESIGN ADVANTAGE



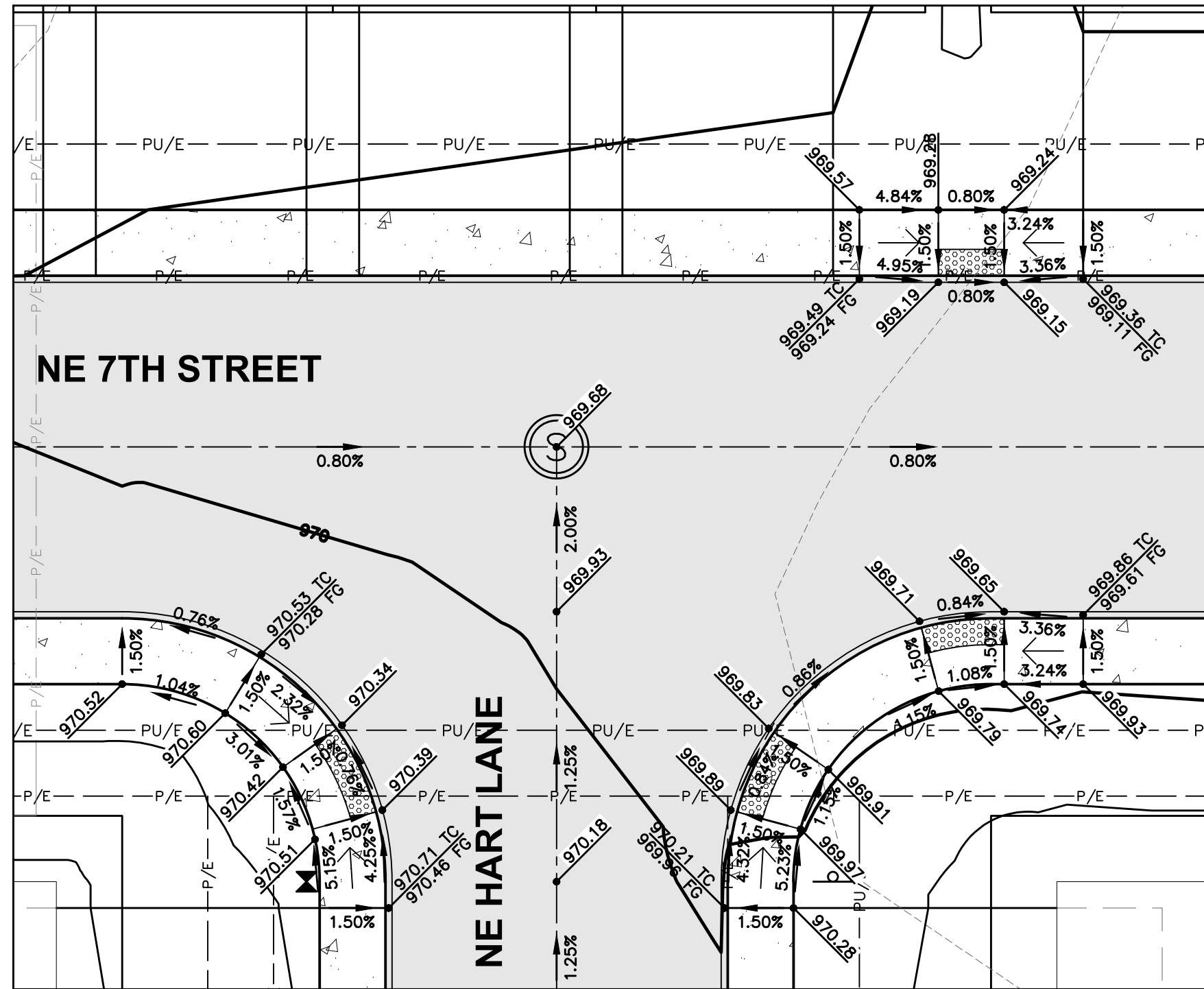
ANKENY, IOWA

DEER CREEK ROWHOMES

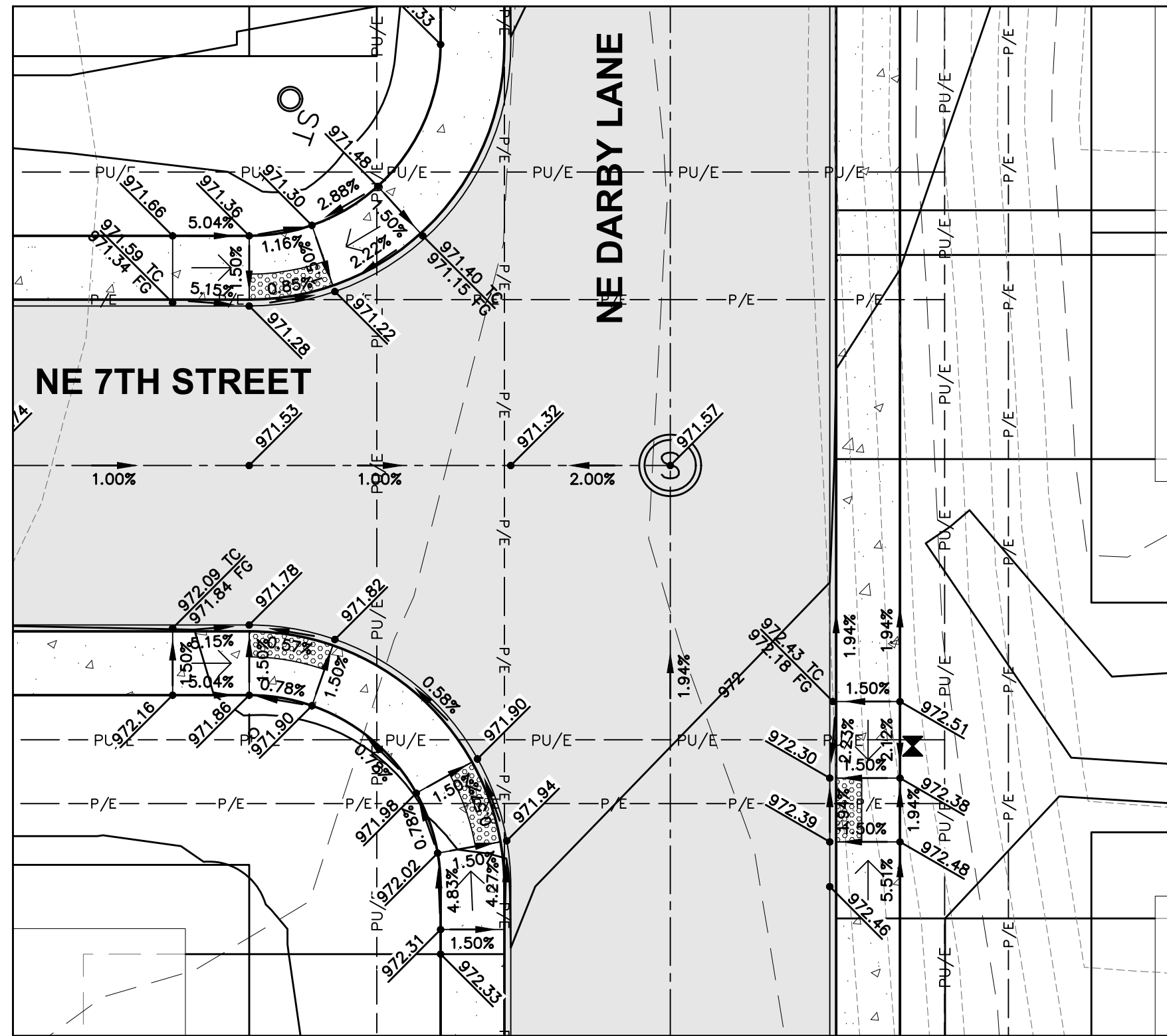
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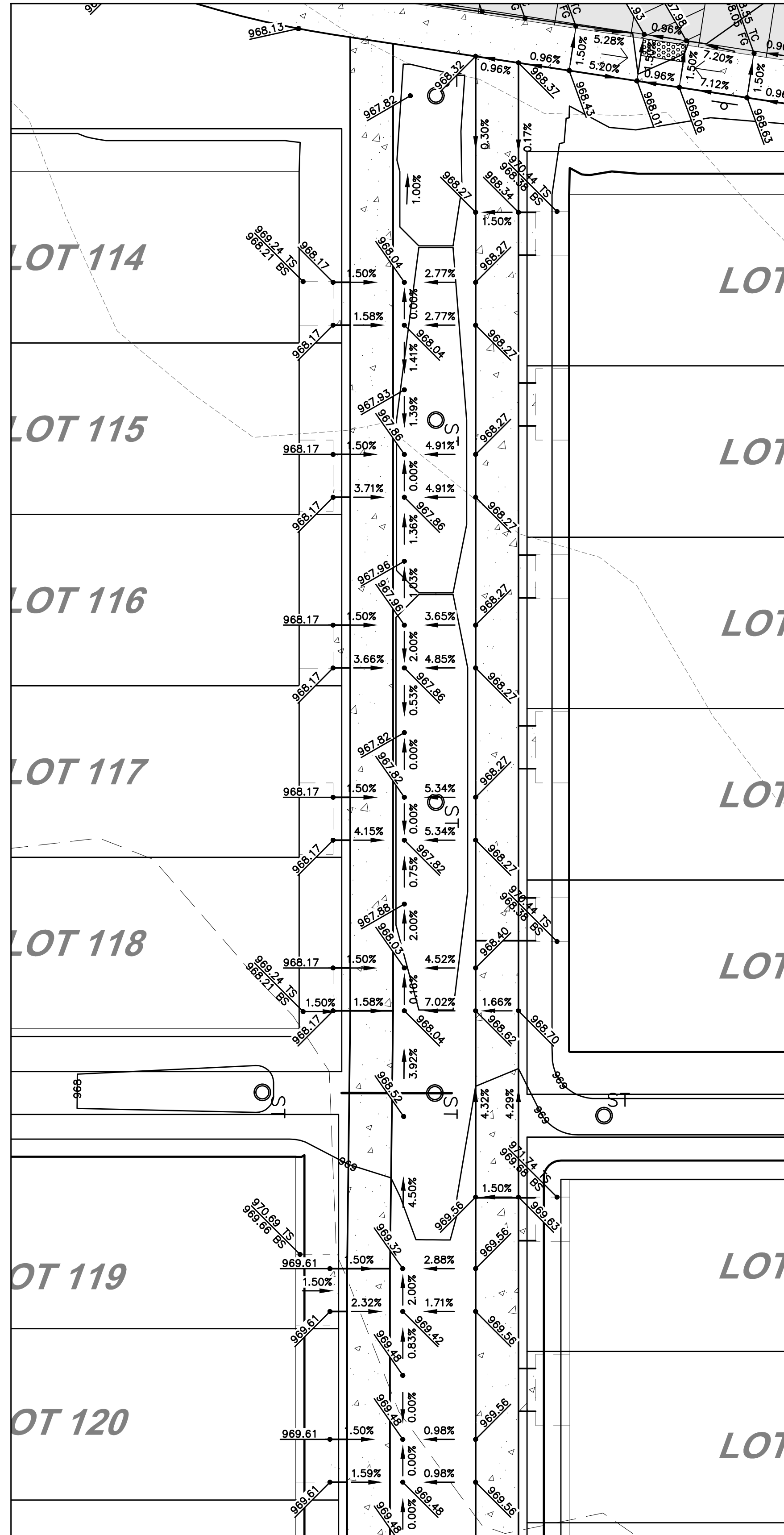
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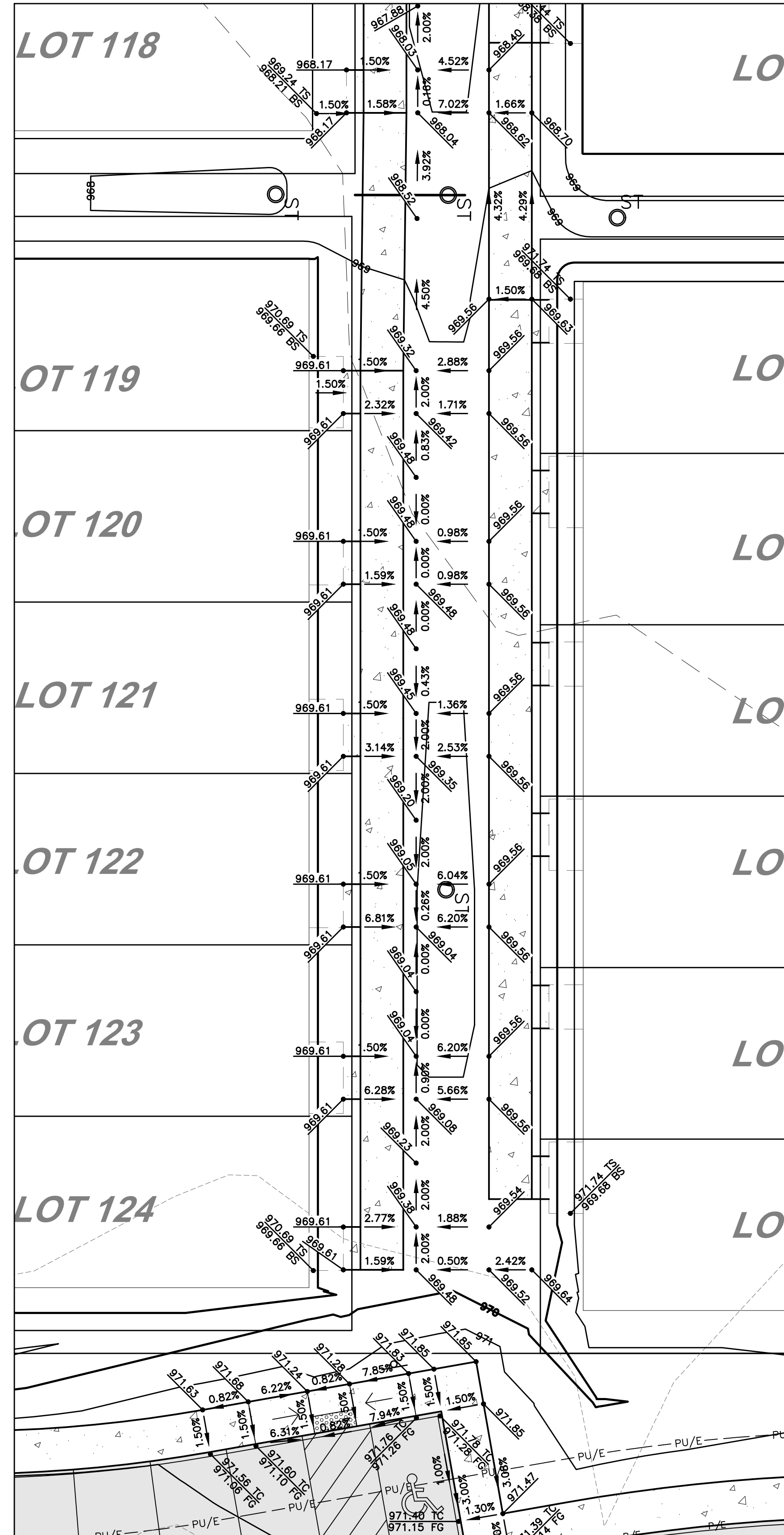
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DETAIL 'G' (1"=10' FULL-SIZE)



DETAIL 'H' (1"=10' FULL-SIZE)



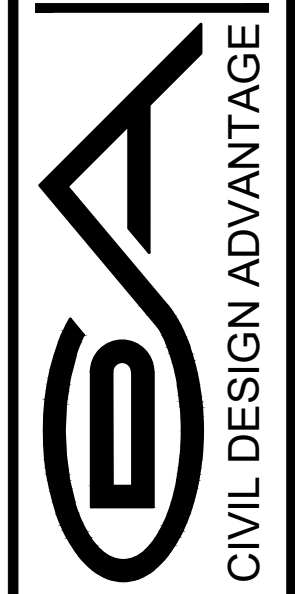
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08/23/2024	SECOND SUBMITTAL
08/02/2024	FIRST SUBMITTAL

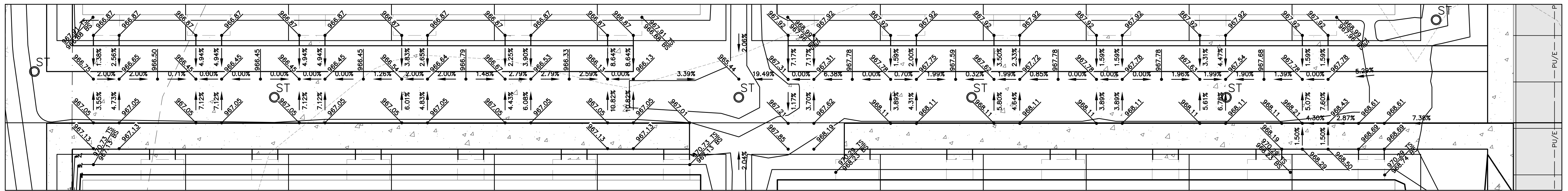
4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 369-4400



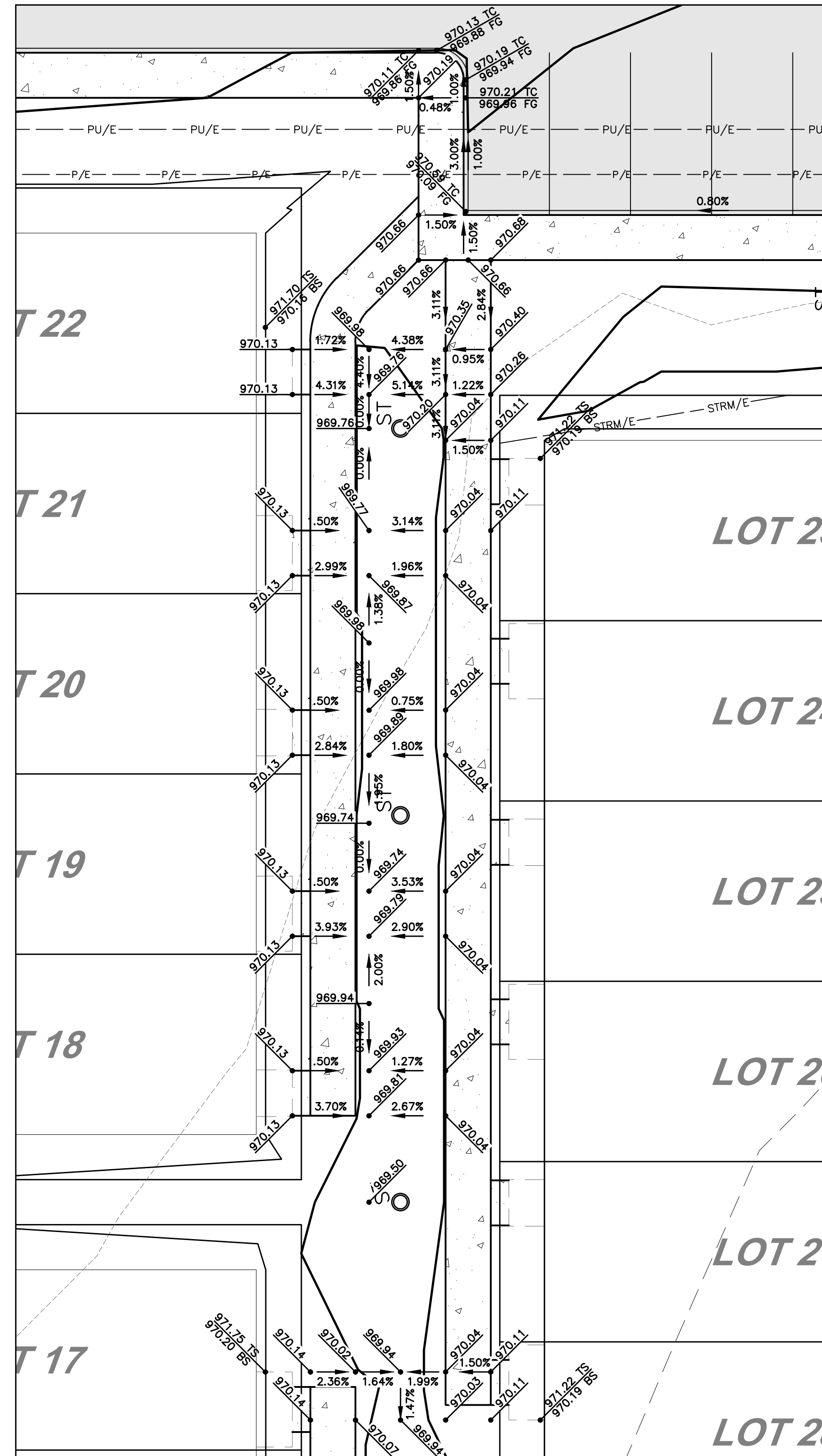
ANKENY, IOWA

DEER CREEK ROWHOMES
GRADING PLAN

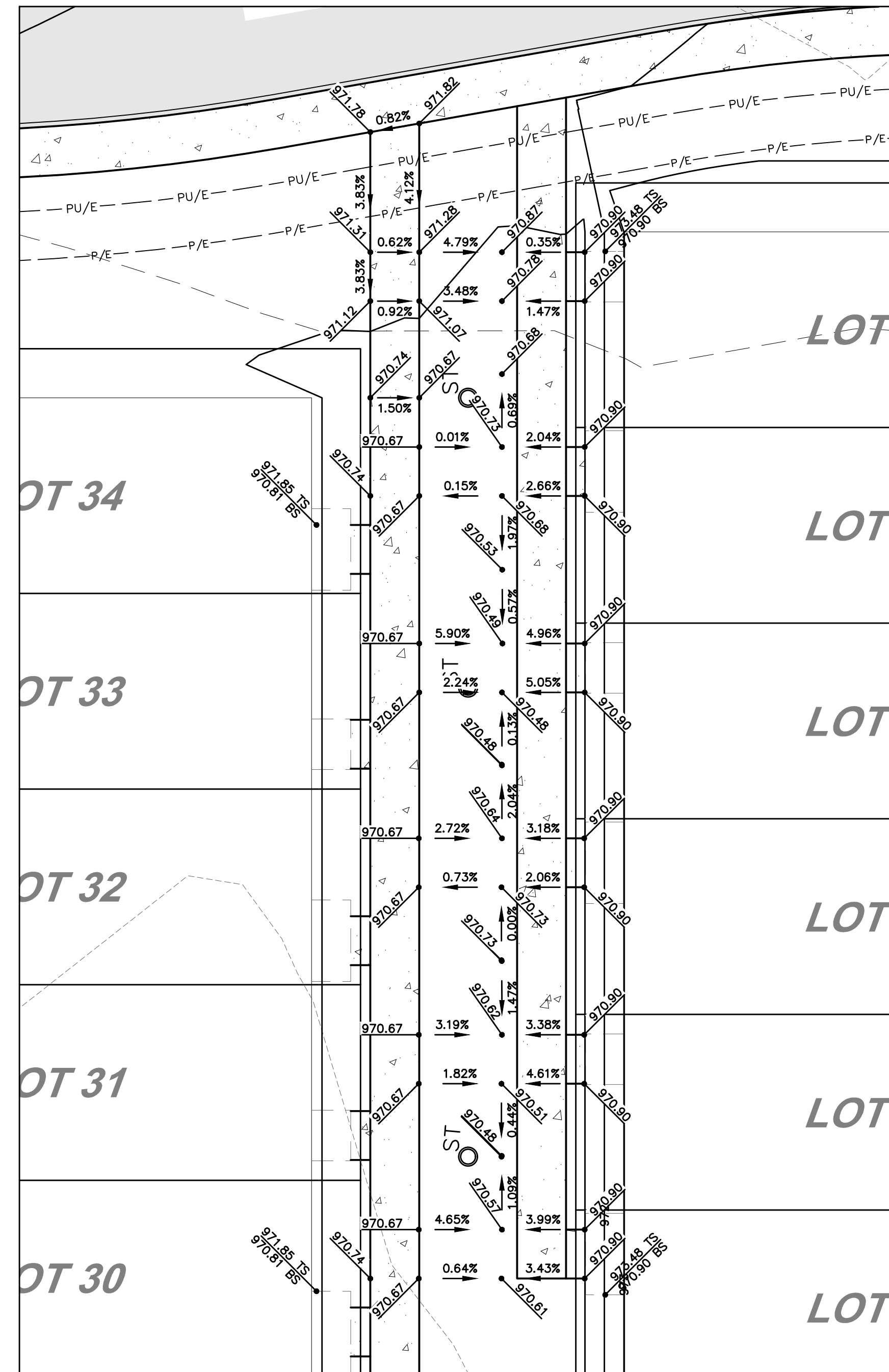
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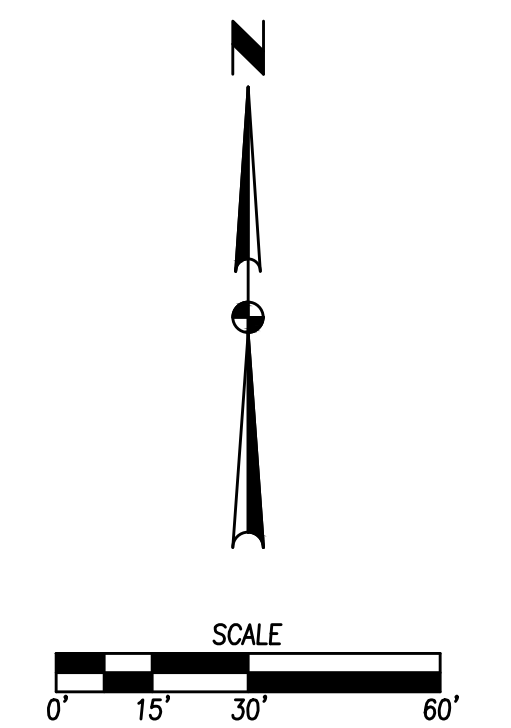
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DETAIL 'K' (1"=10' FULL-SIZE)



DETAIL 'L' (1"=10' FULL-SIZE)



REVISIONS	DATE
THIRD SUBMITTAL	09/06/2024
SECOND SUBMITTAL	08/23/2024
FIRST SUBMITTAL	08/02/2024

4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 369-4400



CIVIL DESIGN ADVANTAGE
 ANKENY, IOWA

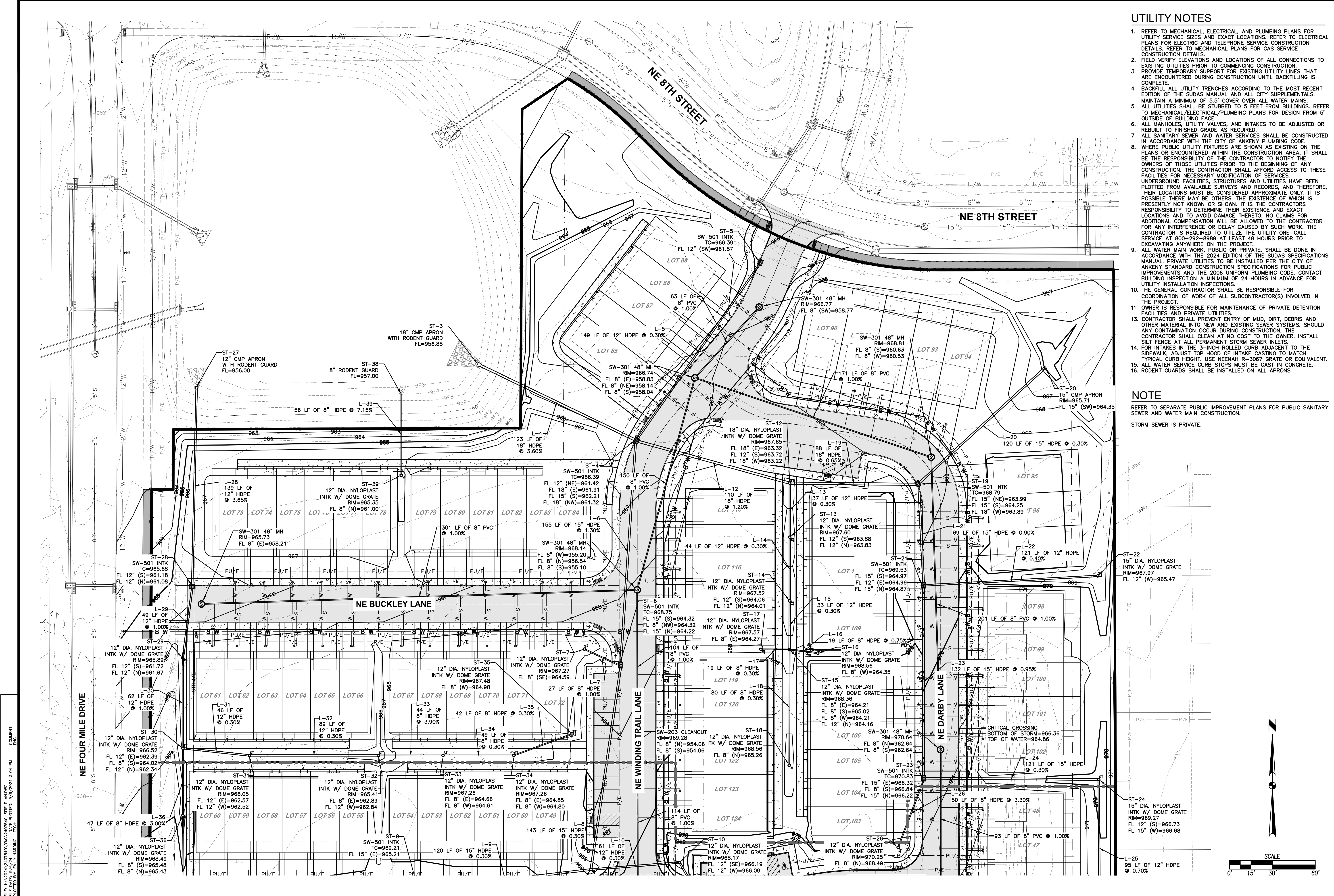
DEER CREEK ROWHOMES
GRADING PLAN

4.4

2407.540

ENGINEER:

TECH:



UTILITY NOTES

- REFER TO MECHANICAL, ELECTRICAL, AND PLUMBING PLANS FOR UTILITY SERVICE SIZES AND EXACT LOCATIONS. REFER TO ELECTRICAL PLANS FOR ELECTRIC AND TELEPHONE SERVICE CONSTRUCTION DETAILS. REFER TO MECHANICAL PLANS FOR GAS SERVICE CONSTRUCTION DETAILS.
- FIELD VERIFY ELEVATIONS AND LOCATIONS OF ALL CONNECTIONS TO EXISTING UTILITIES PRIOR TO COMMENCING CONSTRUCTION.
- PROVIDE TEMPORARY SUPPORT FOR EXISTING UTILITY LINES THAT ARE ENCOUNTERED DURING CONSTRUCTION UNTIL BACKFILLING IS COMPLETE.
- BACKFILL ALL UTILITY TRENCHES ACCORDING TO THE MOST RECENT EDITION OF THE SUDAS MANUAL AND ALL CITY SUPPLEMENTALS. MAINTAIN A MINIMUM OF 5.5' COVER OVER ALL WATER MAINS.
- ALL UTILITIES SHALL BE STUBBED TO 5 FEET FROM BUILDINGS. REFER TO MECHANICAL/ELECTRICAL/PLUMBING PLANS FOR DESIGN FROM 5' OUTSIDE OF BUILDING FACE.
- ALL MANHOLES, UTILITY VALVES, AND INTAKES TO BE ADJUSTED OR REBUILT TO FINISHED GRADE AS REQUIRED.
- ALL SANITARY SEWER AND WATER SERVICES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF ANKENY PLUMBING CODE.
- WHERE PUBLIC UTILITY FIXTURES ARE SHOWN AS EXISTING ON THE PLANS OR ENCOUNTERED WITHIN THE CONSTRUCTION AREA, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE OWNERS OF THOSE UTILITIES PRIOR TO THE BEGINNING OF ANY CONSTRUCTION. THE CONTRACTOR SHALL AFFORD ACCESS TO THESE FACILITIES FOR NECESSARY MODIFICATION OF SERVICES. UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS, AND THEREFORE, THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. IT IS POSSIBLE THERE MAY BE OTHERS. THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN OR SHOWN. IT IS THE CONTRACTORS RESPONSIBILITY TO DETERMINE THEIR EXISTENCE AND EXACT LOCATIONS AND TO AVOID DAMAGE THERETO. NO CLAIMS FOR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR ANY INTERFERENCE OR DELAY CAUSED BY SUCH WORK. THE CONTRACTOR IS REQUIRED TO UTILIZE THE UTILITY ONE-CALL SERVICE AT 800-292-9999 AT LEAST 48 HOURS PRIOR TO EXCAVATING ANYWHERE ON THE PROJECT.
- ALL WATER MAIN WORK, PUBLIC OR PRIVATE, SHALL BE DONE IN ACCORDANCE WITH THE 2024 EDITION OF THE SUDAS SPECIFICATIONS MANUAL. PRIVATE UTILITIES TO BE INSTALLED PER THE CITY OF ANKENY STANDARD CONSTRUCTION SPECIFICATIONS FOR PUBLIC IMPROVEMENTS AND THE 2006 UNIFORM PLUMBING CODE. CONTACT BUILDING INSPECTION A MINIMUM OF 24 HOURS IN ADVANCE FOR UTILITY INSTALLATION INSPECTIONS.
- THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF WORK OF ALL SUBCONTRACTOR(S) INVOLVED IN THE PROJECT.
- OWNER IS RESPONSIBLE FOR MAINTENANCE OF PRIVATE DETENTION FACILITIES AND PRIVATE UTILITIES.
- CONTRACTOR SHALL PREVENT ENTRY OF MUD, DIRT, DEBRIS AND OTHER MATERIAL INTO NEW AND EXISTING SEWER SYSTEMS. SHOULD ANY CONTAMINATION OCCUR DURING CONSTRUCTION, THE CONTRACTOR SHALL CLEAN AT NO COST TO THE OWNER. INSTALL SILT FENCE AT ALL PERMANENT STORM SEWER INLETS.
- FOR INTAKES IN THE 3-INCH ROLLED CURB ADJACENT TO THE SIDEWALK, ADJUST TOP HOOD OF INTAKE CASTING TO MATCH TYPICAL CURB HEIGHT. USE NEEVAH R-3067 GRATE OR EQUIVALENT.
- ALL WATER SERVICE CURB STOPS MUST BE CAST IN CONCRETE.
- RODENT GUARDS SHALL BE INSTALLED ON ALL APRONS.

NOTE
REFER TO SEPARATE PUBLIC IMPROVEMENT PLANS FOR PUBLIC SANITARY SEWER AND WATER MAIN CONSTRUCTION.
STORM SEWER IS PRIVATE.

DATE	REVISIONS
08/02/2024	FIRST SUBMITTAL
08/23/2024	SECOND SUBMITTAL
09/06/2024	THIRD SUBMITTAL

4121 NW URBANDALE DRIVE
URBANDALE, IA 50322
PHONE: (515) 369-4400

TECH: ENGINEER:

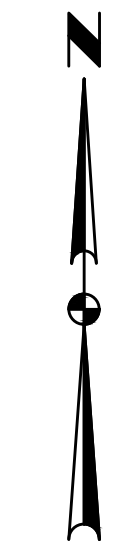
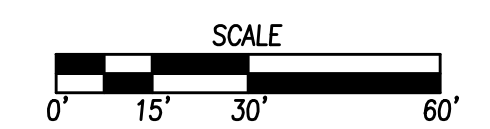
ANKENY, IOWA

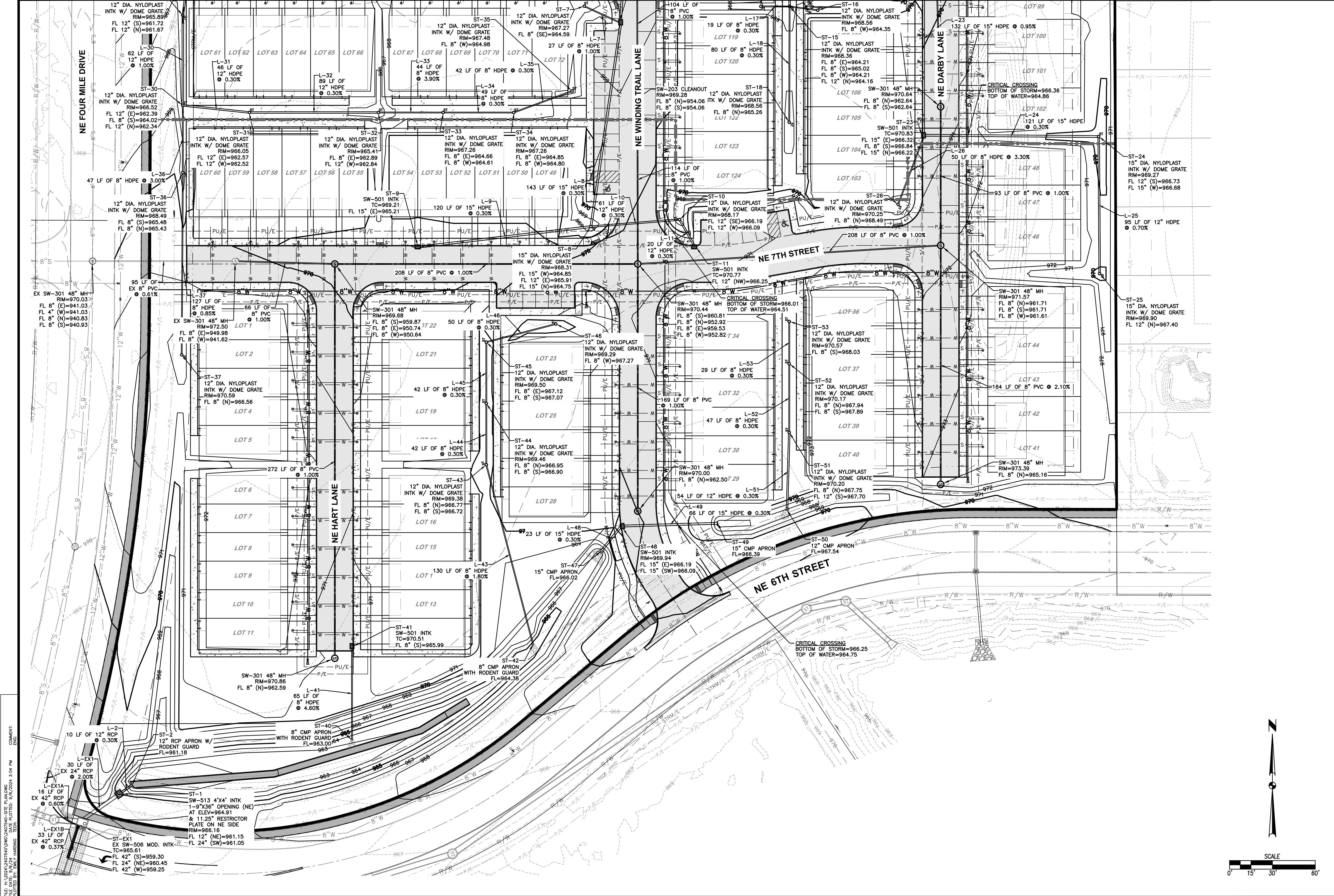
DEER CREEK ROWHOMES
UTILITY PLAN

5.0

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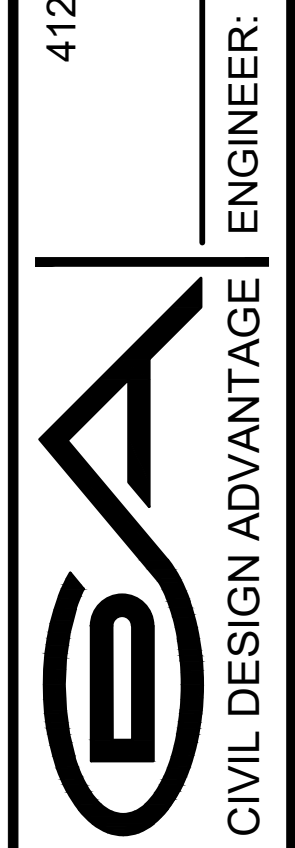




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DATE	REVISIONS
09/06/2024	
08/23/2024	THIRD SUBMITTAL
08/02/2024	SECOND SUBMITTAL
	FIRST SUBMITTAL

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 URBANDALE, IA 50322
 PHONE: (515) 369-4400



CIVIL DESIGN ADVANTAGE
 ANKENY, IOWA

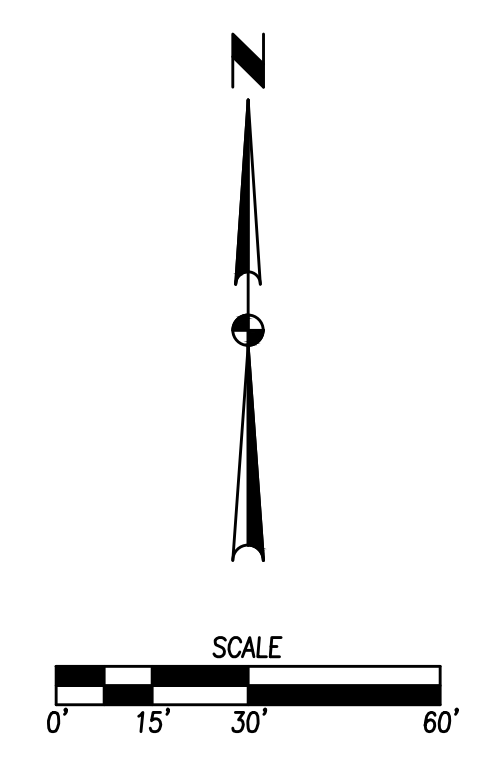
DEER CREEK ROWHOMES

UTILITY PLAN

5.1

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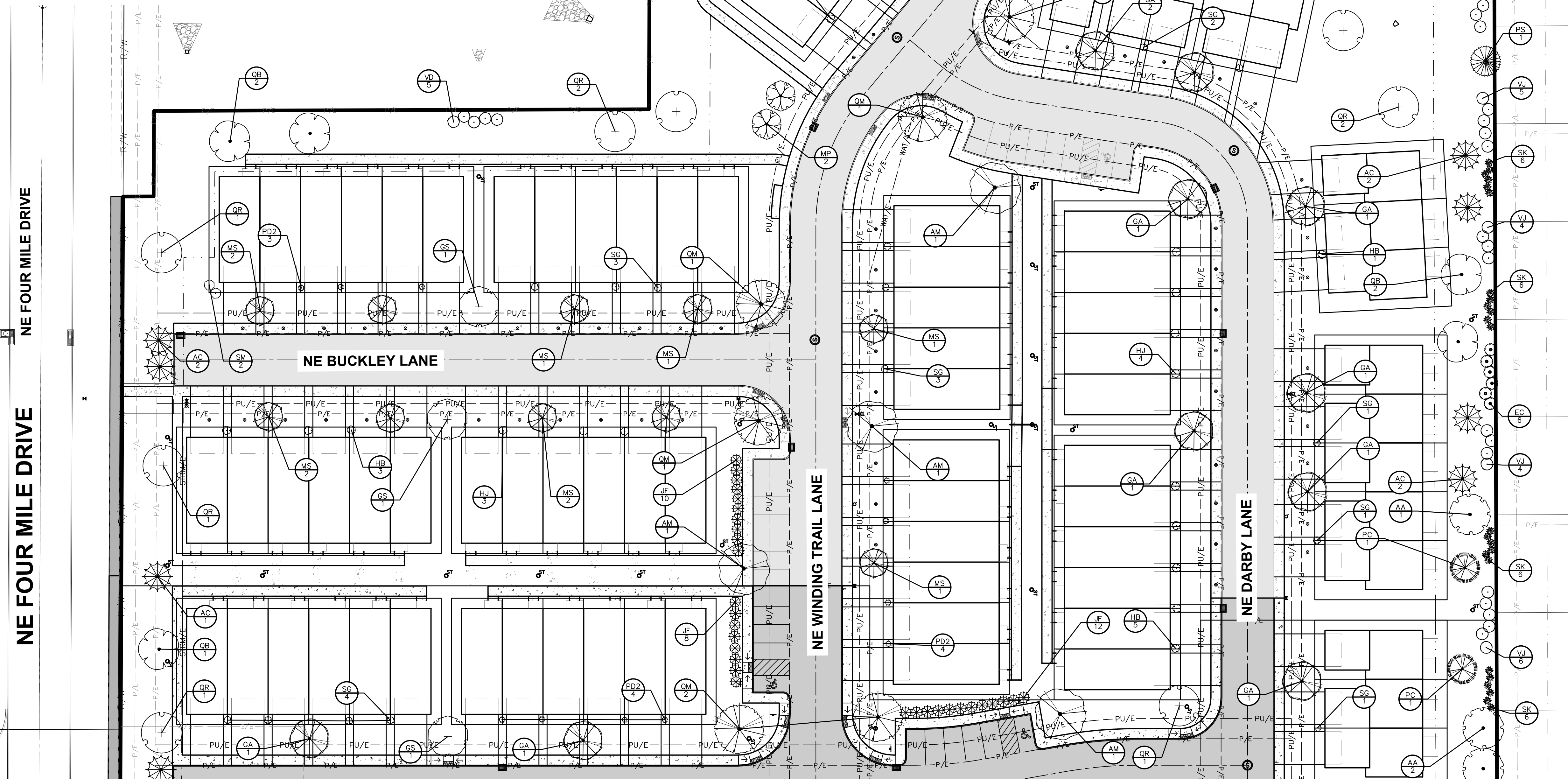
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PLANT SCHEDULE

CODE	QTY	COMMON NAME	BOTANICAL NAME	CONDITION AND SIZE
EVERGREEN TREES				
AC	19	White Fir	Abies concolor	B&B, 6' HEIGHT
PC	16	Colorado Spruce	Picea pungens	B&B, 6' HEIGHT
PD	11	Black Hills Spruce	Picea glauca 'Densata'	B&B, 6' HEIGHT
PS	5	White Pine	Pinus strobus	B&B, 6' HEIGHT
ORNAMENTAL TREES				
MP	12	Prairifire Crab Apple	Malus x 'Prairifire'	B&B, 1.5" CALIPER
MS	26	Spring Snow Crab Apple	Malus x 'Spring Snow'	B&B, 1.5" CALIPER
OVERSTORY TREES				
AA	12	Autumn Blaze Red Maple	Acer rubrum 'Autumn Blaze'	B&B, 2" CALIPER
AM	8	State Street™ Miyabe Maple	Acer miyabei 'Morton'	B&B, 2" CALIPER
BR	3	River Birch	Betula nigra	B&B, 2" CALIPER
GA	12	Autumn Gold Maidenhair Tree	Ginkgo biloba 'Autumn Gold'	B&B, 2" CALIPER
GS	3	Shademaster Honey Locust	Gleditsia triacanthos inermis 'Shademaster'	B&B, 2" CALIPER
QB	7	Swamp White Oak	Quercus bicolor	B&B, 2" CALIPER
QM	15	Burr Oak	Quercus macrocarpa	B&B, 2" CALIPER
QR	15	Red Oak	Quercus rubra	B&B, 2" CALIPER
SHRUBS				
EC	24	Compact Burning Bush	Euonymus alatus 'Compactus'	18" HEIGHT
HB	13	Bobo®Panicle Hydrangea	Hydrangea paniculata 'LVOBO'	18" HEIGHT
HJ	20	Little Lime®Panicle Hydrangea	Hydrangea paniculata 'Jane'	18" HEIGHT
JF	40	Sea Green Juniper	Juniperus chinensis 'Sea Green'	18" HEIGHT
PD2	14	Tiny Wine®Gold Ninebark	Physocarpus opulifolius 'SMNPOTWG'	18" HEIGHT
SG	24	Goldflame Spirea	Spiraea x bumalda 'Goldflame'	18" HEIGHT
SK	42	Miss Kim Korean Lilac	Syringa pubescens 'Miss Kim'	18" HEIGHT
SM	18	Dwarf Korean Lilac	Syringa meyeri 'Palibin'	18" HEIGHT
VO	15	Arrowwood Viburnum	Viburnum dentatum 'Arrowwood'	18" HEIGHT
VJ	24	Judd Viburnum	Viburnum x juddii	18" HEIGHT

LANDSCAPE NOTES AND CALCULATIONS CAN BE FOUND ON SHEET 6.1



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 COMMENT: ENCL
 PLOTTED BY: DAVID HARDING
 DATE: 5/6/2024 3:05 PM

DATE	08/06/2024	08/23/2024	08/02/2024
REVISIONS			
THIRD SUBMITTAL			
SECOND SUBMITTAL			
FIRST SUBMITTAL			

4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 369-4400

TECH: [Blank]
 ENGINEER: [Blank]

DEER CREEK ROWHOMES
LANDSCAPE PLAN

ANKENY, IOWA

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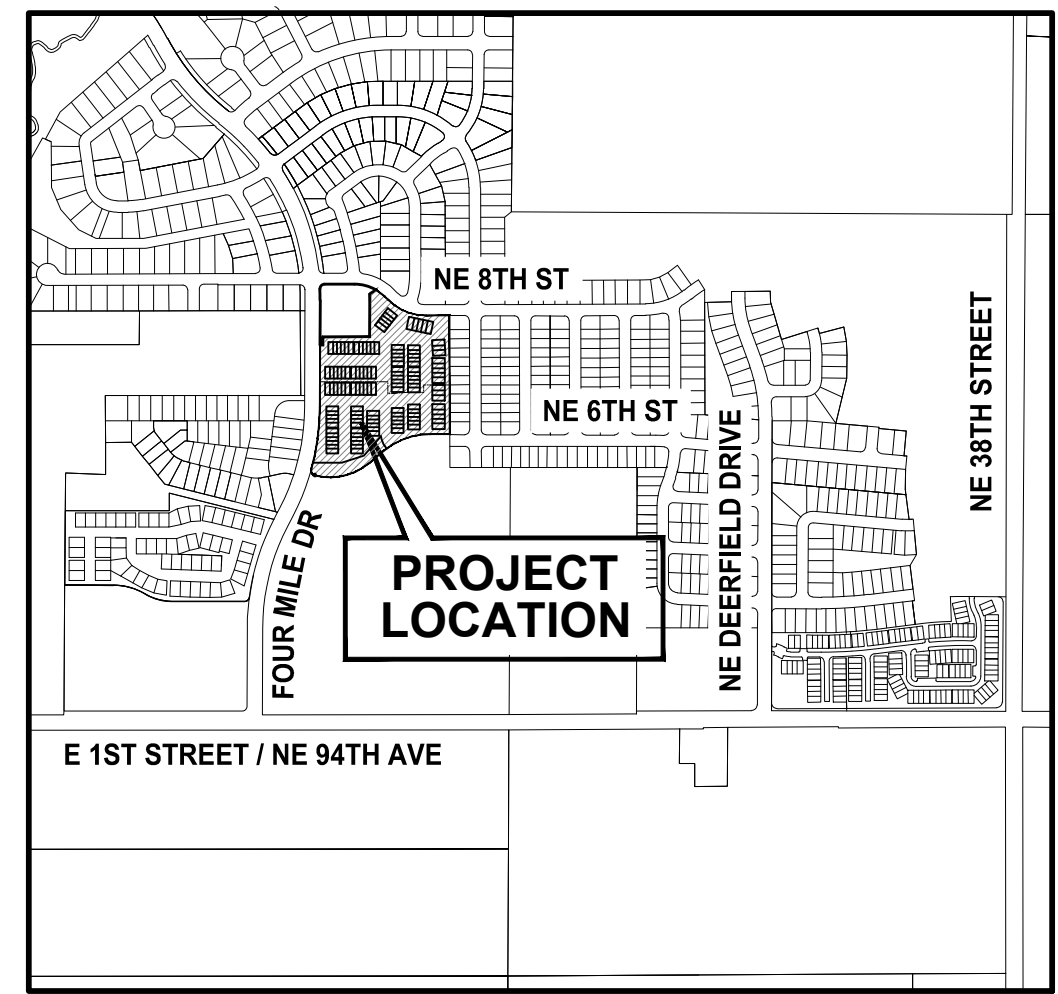
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DEER CREEK ROWHOMES

EROSION AND SEDIMENT CONTROL PLAN

VICINITY MAP

NOT TO SCALE



ANKENY, IOWA

DISCHARGE POINT SUMMARY

DISCHARGE POINT #1 TO DEER CREEK ±2,500 FT		8.01 ACRES
TOTAL AREA DISTURBED TO DISCHARGE POINT		28,836 CU FT
STORAGE VOLUME REQUIRED (# OF ACRES*3600 CU FT)		
VOLUME PROVIDED IN SILT FENCE (1,210 LF @ 10.0 CU FT/LF OF FENCE)		12,100 CU FT
VOLUME PROVIDED IN DITCH CHECK (1,677 LF @ 15.0 CU FT/LF)		25,155 CU FT
TOTAL VOLUME PROVIDED		37,255 CU FT
DISCHARGE POINT #2 TO DEER CREEK ±3,100 FT		3.04 ACRES
TOTAL AREA DISTURBED TO DISCHARGE POINT		10,944 CU FT
STORAGE VOLUME REQUIRED (# OF ACRES*3600 CU FT)		
VOLUME PROVIDED IN SILT FENCE (1,320 LF @ 10.0 CU FT/LF OF FENCE)		13,200 CU FT
VOLUME PROVIDED IN DITCH CHECK (622 LF @ 15.0 CU FT/LF)		9,330 CU FT
TOTAL VOLUME PROVIDED		22,530 CU FT

STABILIZATION QUANTITIES

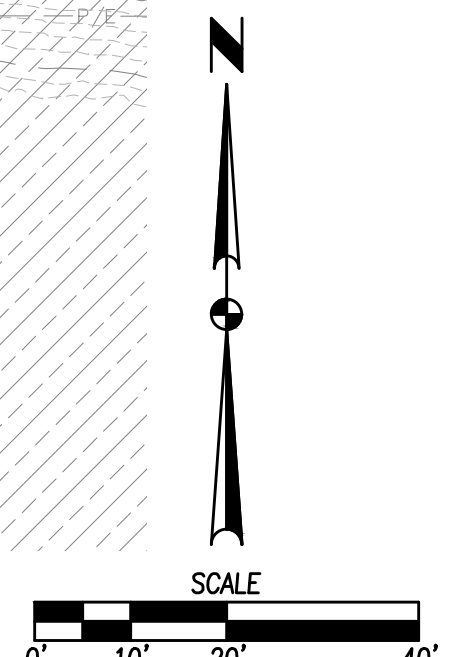
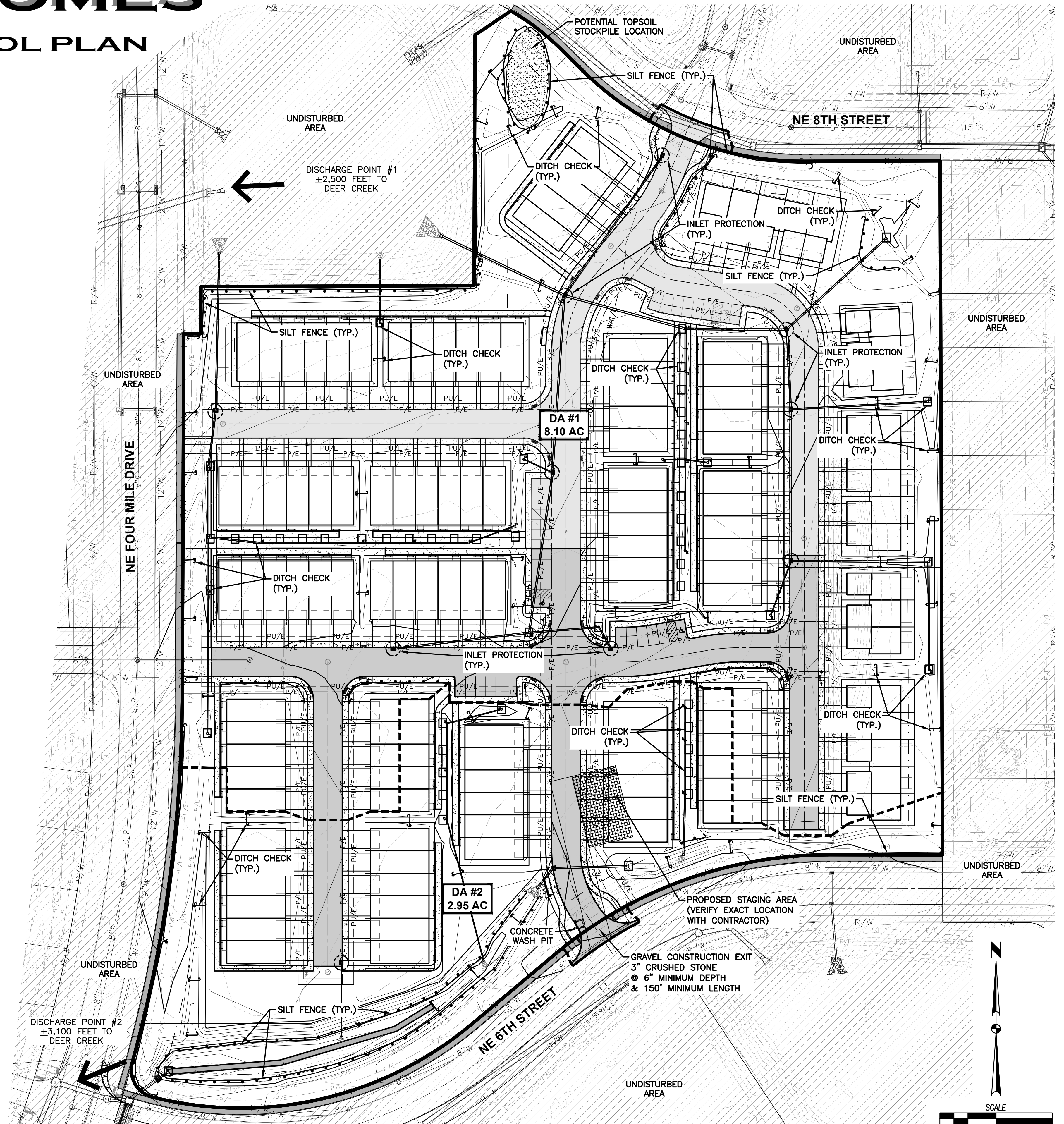
ITEM NO.	ITEM	UNIT	TOTAL
1	SILT FENCE	LF	2,530
2	DITCH CHECKS	LF	2,299
3	SEEDING, FERTILIZING, AND MULCHING	AC	4.82
4	INLET PROTECTION DEVICES	EA	10
5	CONCRETE WASHOUT PIT	EA	1
6	12" PVC TEMPORARY STANDPIPE	EA	1

NOTES:

- IF DEWATERING IS NEEDED FOR ANY REASON, DISCHARGE OF WATER OFFSITE IS TO CONFORM WITH THE GENERAL PERMIT #2 REQUIREMENT.
- DISTURBED AREAS SHALL BE TEMPORARILY SEEDED OR MULCHED IMMEDIATELY WHENEVER CLEARING, GRADING, EXCAVATING, OR OTHER EARTH DISTURBING ACTIVITIES HAVE PERMANENTLY OR TEMPORARILY CEASED AND WILL NOT RESUME FOR A PERIOD EXCEEDING 14 CALENDAR DAYS.
- STORM SEWERS AND DRAINAGE WAYS SHALL BE PROTECTED FROM CONCRETE SLURRY PRODUCED BY SAWCUTTING AND CONCRETE GRINDING.

SWPPP LEGEND

DRAINAGE ARROW		UNDISTURBED AREA	
GRADING LIMITS		RIP-RAP	
SILT FENCE		GRAVEL ENTRANCE	
INLET PROTECTION		STAGING AREA	
PORTABLE RESTROOM		CONCRETE WASHOUT PIT	
TEMPORARY STANDPIPE			



COMMENT: SWPPP
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DATE	REVISIONS
08/06/2024	THIRD SUBMITTAL
08/23/2024	SECOND SUBMITTAL
08/02/2024	FIRST SUBMITTAL

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ANKENY, IOWA
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DEER CREEK ROWHOMES

EROSION AND SEDIMENT CONTROL PLAN

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Front Elevation

